



*Corridor Employment*



*Transit Oriented  
Development*



*Corridor Activity Center*

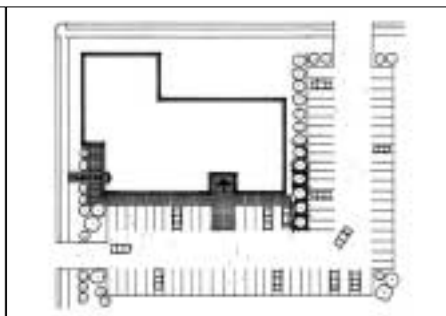
# ROUTE 1 MANUAL

*Howard County, Maryland*

*March 2004*



*Streetscape Design*



*Site Design*



*Building Design*



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This Manual was funded in part by the Maryland Department of Planning, Smart Codes Grant

# TABLE OF CONTENTS

<b>1. INTRODUCTION</b>	<b>1</b>
Background	1
Purpose	3
Authority	3
Required Submission Materials	3
Corridor-wide Objectives and Concepts	4
Manual Organization	6
<b>2. CORRIDOR ZONING DISTRICTS</b>	<b>7</b>
Nonconforming Uses and Noncomplying Design	7
Corridor Employment District	8
Transit Oriented Development District	12
Corridor Activity Center District	16
Continuing Light Industrial Overlay District	20
<b>3. STREETScape DESIGN</b>	<b>21</b>
Design Intent	21
Applicability	22
Road Network	22
Sidewalks and Crosswalks	24
Street Trees	26
Street Furniture and Amenity Areas	27
Pedestrian Street Lights	29
Utilities	29
<b>4. SITE DESIGN</b>	<b>31</b>
Building Location	31
Vehicular Access	32
Parking Areas	34
Loading and Storage Areas	35
Landscape Planting and Screening	36
Trash Enclosures	38
Mechanical Equipment	38
Stormwater Management	39
Lighting	40
Freestanding Signs	40
On-Site Pedestrian Circulation	41
Site Amenities and Features	41

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<b>5. BUILDING DESIGN . . . . .</b>	<b>43</b>
Design Concepts . . . . .	43
Height . . . . .	44
Mass and Articulation . . . . .	44
Roof Design . . . . .	46
Door and Window Openings . . . . .	47
Signs Attached to Buildings . . . . .	48
<b>6. APPLICATION OF THESE STANDARDS . . . . .</b>	<b>49</b>
Introduction . . . . .	49
New Development . . . . .	49
Exemptions . . . . .	49
Existing Improvements . . . . .	50
Interpretation . . . . .	51
<b>APPENDIX A . . . . .</b>	<b>53</b>
Summary Matrix. . . . .	53

# INTRODUCTION

***Purpose:** This chapter provides an overview of the Manual and its purpose. The Introduction also describes the context for creating three new zoning districts. The new districts set out land use patterns that have a more urban character than currently exists in the corridor. This new character should provide a more efficient use of land and, in specific portions of the corridor, create a concentration of mixed uses that promote economic development and are pedestrian-oriented. Because the intent is to establish a more urban character in the corridor, the Manual augments the Zoning Regulations by more fully describing the desired characteristics of the new districts. This chapter also includes a summary of the key land use objectives and design concepts that are the basis of the requirements and recommendations in this Manual.*



*Figure 1.1. **Before:** Current land use patterns in many corridor locations exhibit a hodgepodge of uses, underutilized properties and inconsistent streetscape design.*



*Figure 1.2. **After:** This Manual provides guidance for achieving the land use visions of the Route 1 Corridor Revitalization Study.*

## Background

Howard County's Route 1 corridor has been aging and showing signs of neglect (Figure 1.1). Revitalizing this significant commercial, industrial and residential corridor, which is defined as all property in the County located east of Interstate 95, is important not only to the corridor's residents and businesses, but also to the future growth of the entire County. Revitalization means improving opportunities for new and expanding corridor businesses and promoting redevelopment of older businesses seeking to renovate and adapt for the current market. The need to enhance and expand housing opportunities in the corridor is acknowledged by providing opportunities for added housing in specific locations in the corridor and allowing housing in mixed-use districts.

General Plan 2000 established the need for the revitalization of the County's older communities. The Plan established policies and actions for community conservation and enhancement, and for balanced and phased growth that affect the corridor. Just as the Plan has a 20-year horizon, redevelopment of the corridor, using the requirements and recommendations in this Manual, will also need a 20-year or longer time period.

The revitalization process began with a two-part study conducted by the Department of Planning and Zoning and a citizen's advisory task force appointed by the County Executive. Two Route 1 corridor workshops, open to all interested citizens, solicited the partici-

pants' visual preferences for the physical development of the corridor and expectations on likely changes in the corridor. Using their firsthand knowledge of the corridor and the results from the workshops, the Task Force made recommendations for the corridor (Figure 1.2). These recommendations are listed in the Phase 1 and Phase 2 Reports of the Route 1 Corridor Revitalization Study. These Reports are available on the County's Web site at [www.co.ho.md.us](http://www.co.ho.md.us) by selecting Departments > Planning and Zoning > Community Planning > Route 1 Corridor Revitalization, and from

the Department of Planning and Zoning's public service counter.

One of the significant recommendations of the Phase 2 Report seeks to change land use patterns in portions of the corridor. To implement this recommendation, three new zoning districts: the Corridor Employment-Continuing Light Industrial (CE-CLI) District, the Transit Oriented Development (TOD) District and the Corridor Activity Center-Continuing Light Industrial (CAC-CLI) District will promote new land use patterns

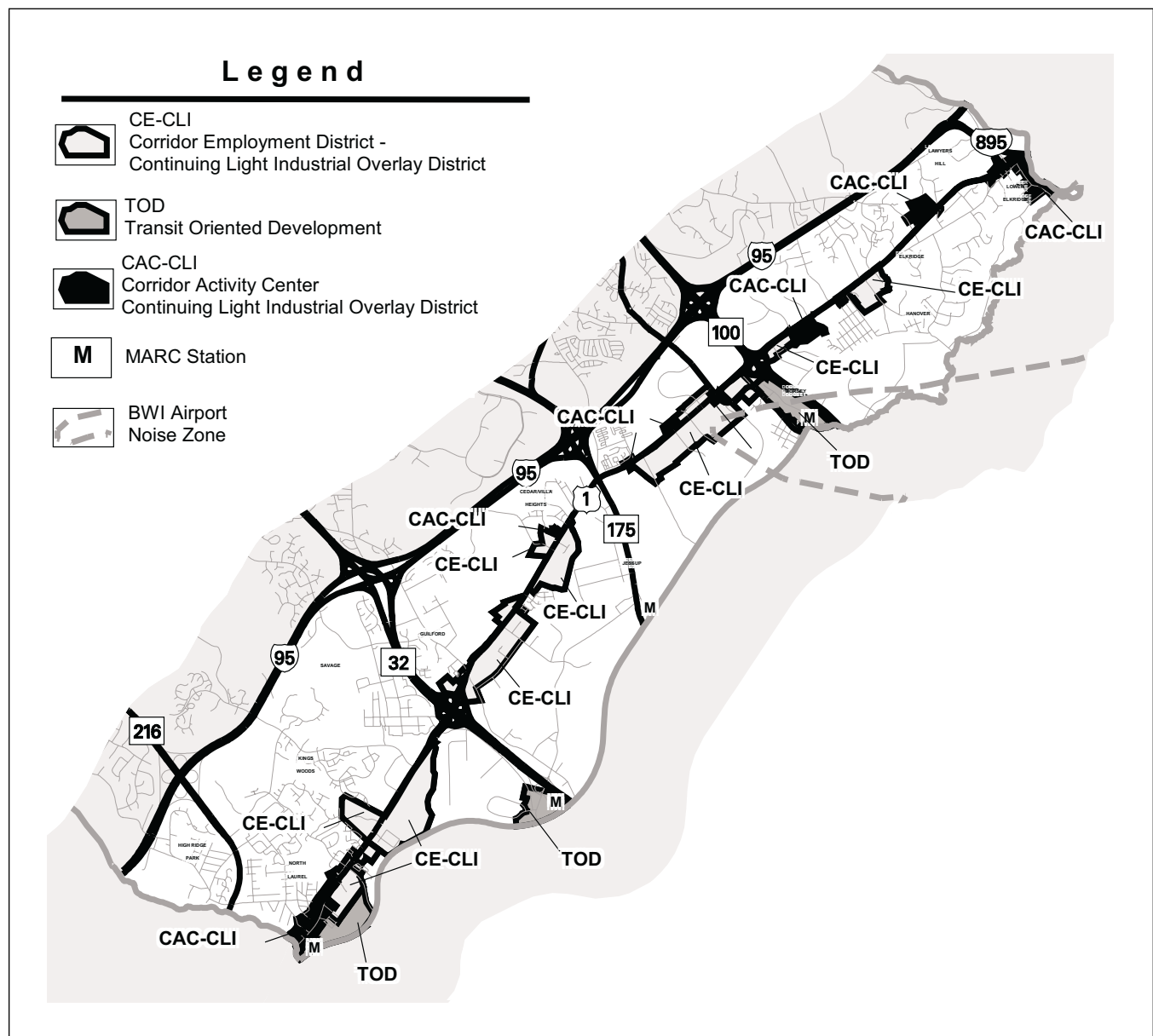


Figure 1.3. The three new zoning districts: the CE-CLI Districts line much of the corridor, the TOD Districts are focused around three key MARC stations and the CAC-CLI Districts are located in the northern, central and southern portions of the corridor, at Elkrige, Jessup and North Laurel.

to better utilize the land and concentrate various uses (Figure 1.3). In addition, the Continuing Light Industrial (CLI) Overlay District was created as a companion to the CE and CAC Districts. The intent of this overlay district is to encourage new land use patterns while still accommodating the existing industrial developments on properties in the CE and CAC Districts.

Only a portion of the corridor has been rezoned using the new districts. Substantial areas of M-1 (Light Manufacturing) and M-2 (Heavy Manufacturing) zoning remain. Commercial retail uses (B-1 and B-2) are still provided in a few places in the corridor. Thus, the corridor will remain the location for a wide range of land uses. Although portions of the Route 1 corridor will be transformed using these new districts, many of the existing industrial and retail land uses will continue.

## Purpose

The Route 1 Manual implements the Zoning Regulations. The Manual provides direction for preparing subdivision and site development plans for properties in the CE-CLI, TOD and CAC-CLI Districts. The Manual's intent is to enhance the image and functioning of the Route 1 corridor through clearly articulated site design and building design requirements and recommendations that apply to these three new districts. Beyond site design and building design, the Manual also has streetscape requirements and recommendations that apply not only to the three new districts, but also to properties in other zoning districts, such as B-1, B-2, M-1 and M-2, that are located adjacent to Route 1.

The purpose of this Manual is to present requirements and recommendations to:

1. Improve the visual appearance of the corridor's streetscape.
2. Enhance the appearance and value of developments in the Route 1 corridor.
3. Establish the desired design character for new developments in the CE, TOD and CAC Districts.
4. Clarify how the Route 1 design requirements and recommendations affect the renovation and expansion of existing uses.
5. Achieve better land use and function by using land more intensively and efficiently.

6. Increase the safety of pedestrians and vehicular traffic, and improve pedestrian and vehicular access to shopping, services, housing and employment.
7. Promote the use of transit and alternative modes of transportation.

This Manual provides illustrations of the design requirements and recommendations for mixed land uses that are envisioned in the three new zoning districts. These graphics and illustrations are intended to help property owners, developers and residents better understand how the proposed changes differ from the existing development patterns. At the same time, they will help guide County reviewers in analyzing development proposals.

## Authority

The Howard County Zoning Regulations and Zoning Map establish the authority and basis for this Route 1 Manual. The Howard County Zoning Regulations, Subdivision and Land Development Regulations, Landscape Manual, Forest Conservation Manual and Design Manual establish site development requirements within the County. The Howard County Sign Code establishes the requirements for installing signs. These documents establish minimum requirements. This Manual establishes additional requirements and recommendations to achieve the design standards for the new zoning districts. Where the requirements of the Route 1 Manual exceed or are more restrictive than those of other documents, the requirements of this Manual shall apply.

State agencies also have jurisdiction and authority over development within the corridor. The State Highway Administration establishes requirements for right-of-way design and site access on State highways. The Maryland Aviation Administration establishes requirements for development within the BWI Airport height and noise zones.

## Required Submission Materials

All plan submissions, beginning with the initial subdivision or site development plan, shall show all applicable streetscape, site and building designs responding to this Manual's requirements and recommendations. To clearly demonstrate that the requirements and recom-

mendations of this Manual are addressed, a written summary of how the proposed design meets the applicable objectives of the Route 1 Manual shall accompany all plan submissions. To show building design, schematic architectural elevations must be included on subdivision or site development plans, as applicable.

## Corridor-wide Objectives and Concepts

The character of the corridor is diverse and includes a rich mix of uses that have evolved over time. Although diversity is to be encouraged and celebrated, this Manual identifies land use objectives and design concepts that can establish a cohesive new vision and encourage better use of the land. Because the corridor has been showing signs of aging and neglect, many of the objectives are focused on transforming the negative characteristics. At the same time, the objectives and concepts also outline a strategy that is intended to make the corridor a thriving and attractive place to live, work, shop and spend leisure time. Figures 1.4 through 1.8 depict many of the objectives and concepts described below.

### Primary Land Use Objectives

1. Create opportunities for new economic development and for more concentrated multi-use development (Figure 1.4).
2. Recognize the need for renovation and expansion of existing businesses.
3. Institute County capital projects and economic development programs that support redevelopment and renovation.
4. Encourage land assembly by allowing more commercial uses if parcels are consolidated.
5. Through the Corridor Employment District, encourage redevelopment for areas along the Route 1 roadway that will use land more efficiently and attractively for office, flex and technology oriented, and light industrial uses. De-emphasize truck-oriented and strip commercial uses.



Figure 1.4. Expansion of employment uses will contribute to the economic health of the corridor and the County.



Figure 1.5. Transit oriented development means convenient access to train and bus routes with attractive train stations and bus shelters.



Figure 1.6. Successful pedestrian-oriented mixed-use development needs an attractive streetscape with wide sidewalks, street trees and street furniture.





*Figure 1.7. Multistory buildings make more efficient use of scarce land. When buildings are located close to the street, with on-street parking and with parking lots to the rear or side, pedestrian access is more convenient.*



*Figure 1.8. Pedestrian connections among buildings and outdoor amenity spaces help make new residential and commercial developments vibrant, active places.*

6. Through the Transit Oriented Development District, offer opportunities in areas next to the MARC stations for denser, more concentrated office, residential and related commercial development to capitalize on transit access (Figure 1.5).
7. Through the Corridor Activity Center District, concentrate areas of pedestrian-oriented commercial, office and residential development in certain locations that complement nearby residential communities (Figure 1.6).

### ***Key Design Concepts***

1. Improve the Route 1 right-of-way by the addition of landscaped medians and streetscape elements such as sidewalks, crosswalks, street trees, street furniture and lighting.
2. Delineate specific areas, primarily in the TOD and CAC Districts, for pedestrian-oriented mixed-use developments (Figure 1.7).
3. Provide convenient vehicular and pedestrian access to transit, both MARC train and bus.
4. Orient buildings to the street, especially along Route 1. Locate parking to the side and rear of buildings with loading and outdoor storage to the rear.
5. Reduce setbacks and promote multistory buildings to make more efficient use of scarce land.
6. In the TOD and CAC Districts, institute on-street parking in areas where roadway improvements will occur.
7. Create attractive and vibrant public places. Integrate amenity spaces, which are open to the public, into developments with new buildings (Figure 1.8).
8. Provide pedestrian and vehicular connections between adjacent commercial uses and to parking lots.
9. Design buildings, site features and streetscapes that will promote safety for residents, workers and visitors in the corridor.

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## Manual Organization

As described above, this Manual establishes the general objectives for the three new zoning districts and then proceeds to more specific aspects of the corridor, setting forth requirements and recommendations for new development and redevelopment. The requirements for the streetscape, site and building design elements generally correspond to requirements of the Zoning Regulations. They represent the most important aspects of the new land use patterns. The recommendations for the design elements represent desired elements that should be provided wherever possible. Developers must address the requirements and are strongly encouraged to address the recommendations.

This Manual is organized by each topic in the following chapters:

**Chapter 1, Introduction.** The Manual begins by presenting the context for the corridor's revitalization. This chapter introduces the overall objectives and concepts for the revitalization of the corridor and the three new zoning districts.

**Chapter 2, Corridor Zoning Districts.** This chapter describes each district's land use goals and design concepts, and offers illustrations of recommended

building types and conceptual site plans that could transform properties to the new patterns.

**Chapter 3, Streetscape Design.** For each element of the streetscape design, a list of requirements and recommendations is included to help public and private sector developers, designers and engineers understand and achieve the desired character for rights-of-way in the corridor. Streetscape design addresses the road right-of-way, but pays primary attention to safety and amenity improvements that affect pedestrians.

**Chapter 4, Site Design.** This chapter presents requirements and recommendations that give the desired characteristics for site design, such as building location, parking, loading, landscaping and pedestrian amenities.

**Chapter 5, Building Design.** Establishing requirements and recommendations for building design is a new concept in the County, created in response to citizens' great concerns about improving the appearance of the corridor. Special emphasis is placed on buildings that front on the Route 1 right-of-way.

**Chapter 6, Application of these Standards.** Because the corridor is substantially built, many properties will not be able to fully comply with these new requirements. Therefore, this chapter sets priorities for compliance with the new requirements in this Manual.

## CORRIDOR ZONING DISTRICTS

***Purpose:** This chapter describes the purpose, land use goals and design concepts for each of the three new zoning districts. Because these three new zoning districts intend to change the existing land use patterns, the text and illustrations in this chapter are meant to help developers and citizens envision the desired development that could occur in these districts.*



Figure 2.1. **Before:** Existing business, manufacturing and industrial uses in the corridor will continue.



Figure 2.2. **After:** Over time, however, some of these uses may be expanded or redeveloped in ways that bring them into compliance with the requirements and recommendations of this Manual.

### Nonconforming Uses and Noncomplying Design

Many parcels in the corridor were developed before these new Corridor Employment (CE), Transit Oriented Development (TOD) and Corridor Activity Center (CAC) Districts were created (Figure 2.1). For the CE and CAC Districts, the Continuing Light Industrial (CLI) Overlay District was created to accommodate existing warehouse and industrial buildings. Because most properties in the CE and CAC Districts are already developed, many with industrial buildings that represent substantial investments, an accommodation needed to be made so that these uses and buildings do not all become nonconforming and noncomplying with the new zoning districts. The CLI Overlay District is this accommodation.

Neither the Howard County Zoning Regulations nor this Manual disallow the continued use of sites developed prior to the adoption of these new districts. Compliance with the requirements and recommendations for these districts will be achieved over time as existing uses are expanded or redeveloped (Figure 2.2). Refer to Chapter 6, Application of These Standards, for more discussion on nonconforming uses.

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## Corridor Employment District

### *Purpose*

The Corridor Employment (CE) District intends to encourage more efficient use of vacant or underutilized land adjacent to Route 1. Development in the CE District should provide for new office, flex space, technology-oriented and light industrial uses that advance the County's economic development goals, while reducing the spread of strip commercial development and encouraging consolidation of fragmented parcels. The CE District requirements should improve the appearance of the Route 1 streetscape, enhance traffic safety and better accommodate public transit and pedestrian travel.

### *Land Use Goals*

1. Encourage redevelopment on sites adjacent to Route 1 for new office, flex space and technology oriented, light industrial employment growth.
2. Encourage renovation of older, obsolete facilities and redevelopment of underutilized land.
3. Promote multistory buildings by allowing certain retail and service commercial uses in multistory buildings.
4. Encourage land assembly by allowing some free-standing commercial uses for redevelopment projects that exceed 20 acres.
5. Reduce the spread of strip commercial development by not allowing most highway-oriented retail uses. Limit truck-oriented uses and uses that require outdoor storage.
6. Allow continuing and nonconforming uses, but define limits to the expansion of buildings and sites.

Help bring existing properties into compliance with the Route 1 Manual over time.

### *Design Concepts*

1. Place buildings close to the street. Allow greater development intensity by establishing building setbacks of 20 feet from the street right-of-way.
2. Construct buildings with multiple stories. Allow heights up to 60 feet with the 20-foot setback from the street. Allow taller buildings with a greater setback from the street or by recessing upper stories.
3. Promote parking to the side or rear of buildings and limit the amount of frontage on Route 1 that can be devoted to parking lots. Establish a maximum building setback line to further limit the potential for parking between the building and the right-of-way.
4. Emphasize streetscape improvements with required sidewalks and street trees.
5. Encourage building facades that orient to Route 1 and have well-defined entrances along Route 1.
6. Require access improvements and circulation designs that enhance traffic safety and accommodate transit and pedestrian travel.

The photographs in Figure 2.3 show several examples of desirable building types for the CE District. Variety in architecture and the presence of multistory buildings, where possible, add visual interest. Figures 2.4 and 2.5 show before and after illustrations, and give an example of how a property could be transformed to a new CE District pattern. The conceptual site plan, Figure 2.6, illustrates elements of the new pattern such as reducing the building's setback from the street and placing parking to the side or rear of the building. The CE building types, before and after images, and site plan were developed with consultant ERM, Inc.

Figure 2.3. Preferred Building Types for the CE District



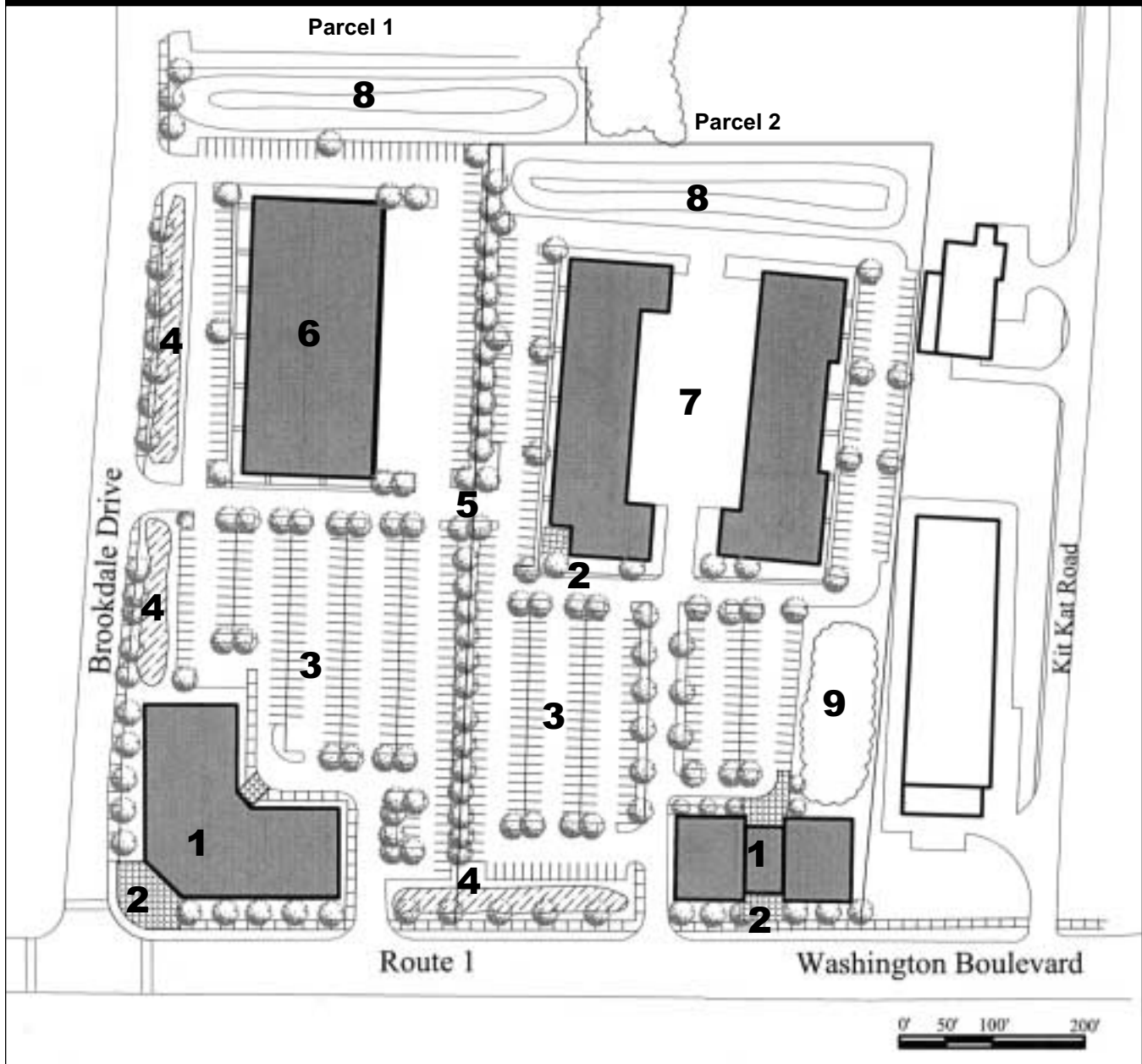


Figure 2.4. **Before:** Existing patterns in the CE District are often typified by one-story buildings with deep setbacks from the road. Streetscape improvements and pedestrian access are often limited.



Figure 2.5. **After:** The CE regulations are intended to foster new patterns that result in multistory buildings close to the right-of-way with parking to the side and rear. Sidewalks and street trees will contribute to an enhanced pedestrian environment.

**Figure 2.6. Concept Plan for CE District**



**Legend**

- |   |  |
|---|--|
| 1. Multistory office building with ground floor retail              | 6. Flex space building with single bay of parking in front and loading in back         |
| 2. Pedestrian amenity area  | 7. Flex space buildings with shared rear loading areas                                 |
| 3. Parking to the side and rear of building                         | 8. Stormwater management area  |
| 4. Infiltration or bioretention area in parking setback to street   | 9. Existing forest retained in partial fulfillment of forest conservation requirements |
| 5. Interconnected parking lots for access between development sites |  |

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## Transit Oriented Development District

### *Purpose*

The Transit Oriented Development (TOD) District intends to encourage the development and redevelopment of key parcels of land within 3,000 feet of a MARC station. Development in the TOD District should provide for multistory office centers that are located and designed for safe and convenient pedestrian access by commuters using the MARC trains and other public transit links. For larger sites of at least five acres, well-designed multi-use centers combining office and high density residential development are encouraged.

### *Land Use Goals*

1. Encourage redevelopment on select sites that are near MARC stations so that workers and residents will use the commuting potential of the MARC line. Create attractive multi-use centers combining multistory office and residential uses.
2. Promote multistory buildings by allowing certain retail and service commercial uses in multistory buildings.
3. Preclude intensive truck-oriented uses and highway-oriented commercial uses.
4. Encourage land assembly by allowing more commercial uses for redevelopment projects that exceed 15 acres.
5. Allow residential development on up to 50% of the TOD land with a 15% moderate income housing unit requirement.

### *Design Concepts*

1. Orient buildings and sites to the major pedestrian-oriented streets, especially those that give access to the MARC station.
2. Bring buildings close to the street through zero-foot building setbacks along roads that serve as major pedestrian access routes to MARC stations.
3. Require wide sidewalks on main routes. Encourage pedestrian-oriented first floor retail uses.
4. Encourage the development of attractive and comfortable amenity areas for workers and residents.
5. Use height, setback and parking regulations to limit density rather than establishing a maximum density.
6. Encourage safe and convenient pedestrian travel between the TOD uses and the MARC station.

The photographs in Figure 2.7 show several examples of desirable building types for the TOD District. To take advantage of the location next to MARC stations, the buildings are multistory, thus allowing denser development. The buildings have articulated facades that increase their visual interest to passers-by. Figures 2.8 and 2.9 show before and after illustrations, and give an example of how a property could be transformed to a new TOD District pattern. The conceptual site plan, Figure 2.10, illustrates elements of the new pattern such as reducing the building's setback from the street and placing the parking in structures or away from the street. The TOD building types, before and after images, and site plan were developed with consultant ERM, Inc.



Figure 2.7. Preferred Building Types for the TOD District





Figure 2.8. **Before:** Existing patterns in the TOD District are often typified by large surface parking lots and an assortment of auto-oriented uses and storage yards.

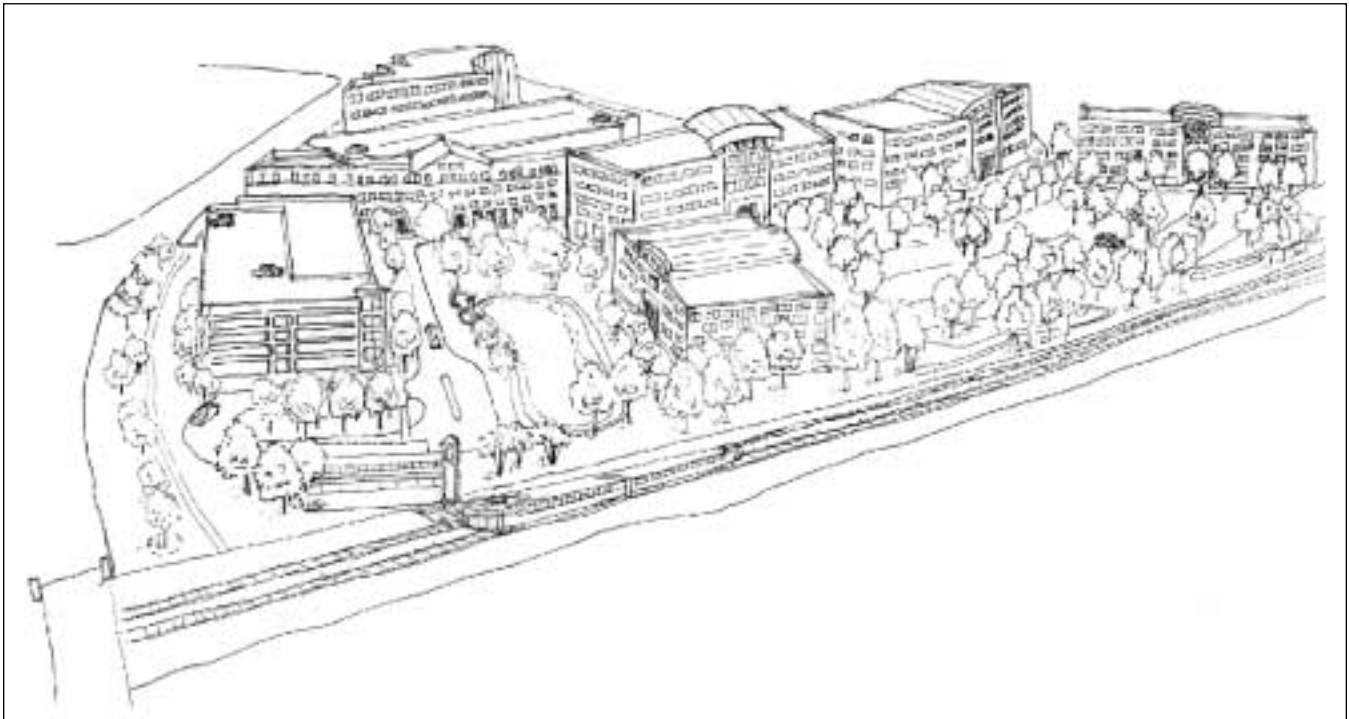


Figure 2.9. **After:** The TOD regulations are intended to foster high density office and residential development. Structured parking and accessory retail uses, streetscape improvements and amenity outdoor areas will create attractive working and living environments.

**Figure 2.10. TOD Conceptual Site Plan**



**Legend**

- |   |  |
|---|--|
| 1. Train station  | 7. Surface parking or potential future garage parking                                  |
| 2. Bus drop-off and “kiss and ride” parking area                              | 8. Stormwater infiltration area  |
| 3. Parking garage with ground floor retail and office use                     | 9. Residential multistory apartment building with ground floor retail and service uses |
| 4. Stormwater management area/amenity area                                    | 10. Restaurant with offices above  |
| 5. Pedestrian-oriented main street with wide sidewalks and zero-foot setbacks | 11. Parking garage   |
| 6. Office building with ground floor retail                                   | 12. Office building  |
|   | 13. Pedestrian amenity area  |

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## Corridor Activity Center District

### *Purpose*

The Corridor Activity Center (CAC) District intends to provide for the development of pedestrian-oriented, urban activity centers with a mix of retail, service, office and residential uses. These centers should be located adjacent to Route 1 and close to residential communities that will benefit from a pedestrian-oriented local business area. The CAC District requirements should result in renovation and redevelopment that will strengthen nearby communities, provide for safe and convenient pedestrian travel, and improve the streetscape of Route 1 and intersecting streets.

### *Land Use Goals*

1. Encourage multistory buildings that adjoin public sidewalks.
2. Promote first floor retail and service uses on the Route 1 frontages of buildings with office and housing above. At least 50% of the first floor on Route 1 should be retail or service uses. An individual retail business cannot exceed 20,000 square feet of floor area.
3. Reduce the spread of strip commercial development by not allowing most highway-oriented retail uses.
4. Limit uses that are truck-oriented and that require outdoor storage.
5. Allow for housing at a density of up to 25 units per net acre with a 15% moderate income housing requirement on sites of at least two acres.

### *Design Concepts*

1. Promote pedestrian-oriented streetscapes by requiring building facades that extend along at least

75% of the property's Route 1 frontage. Provide pedestrian interest by the presence of windows, doors and architectural features. Relate the building facades to the sidewalks through a minimum zero-foot and a maximum 10-foot setback from the design right-of-way.

2. Promote parking to the rear and side of the buildings. Encourage shared access and interconnected parking between adjacent properties.
3. Require pedestrian-oriented improvements that include wide sidewalks, crosswalks, street trees, street furniture and lighting.
4. In areas of distinct historic character, such as Lower Elkridge, encourage new buildings to be compatible with existing buildings in height, mass and articulation.
5. Design the exterior walls of new buildings with different building planes, colors or materials, and/or with small setbacks, indentations or other architectural means that provide visual interest at a pedestrian scale. For new buildings at prominent intersections along the Route 1 roadway, add architectural details for emphasis at the most visible corner(s).

The photographs in Figure 2.11 show several examples of desirable building types for the CAC District. The multi-use buildings and the more urban type of housing are appropriate for this district. Figures 2.12 to 2.15 show before and after illustrations, and give examples of how properties could be transformed to the new CAC District pattern. The conceptual site plan, Figure 2.16, illustrates elements of the new pattern such as eliminating the building's setback from the design right-of-way and placing parking away from the street.

The North Laurel Concept Plan, developed by consultant A. Nelessen Associates, is the inspiration for activity center site design and architectural massing.

Figure 2.11. Preferred Building Types for the CAC District





Figure 2.12. **Before:** Over time, the commercial activity along portions of the Route 1 corridor has declined, leaving vacant sites, freestanding uses and strip centers set back from the road.



Figure 2.13. **After:** Concentrated areas of mixed uses with ground floor retail and upper story office and residential uses can transform parts of the corridor into vibrant activity centers.



Figure 2.14. **Before:** Existing restaurants and retail uses along Route 1 lack the streetscape amenities that could make them more attractive community-oriented pedestrian destinations.



Figure 2.15. **After:** Sidewalks, street trees and on-street parking make retail and commercial areas more inviting and convenient. Expansion and redevelopment that introduces multistory buildings can add to the economic viability of these areas.

Figure 2.16. Concept Plans for CAC District



Phase 1

Phase 2



**Legend**

- |  |   |
|--|---|
| 1. Multistory building with ground floor retail and upper floors office or residential | 3. Parking (P) on street and to rear of buildings |
| 2. Pedestrian amenity area centrally located   | 4. Multistory residential buildings               |

## Continuing Light Industrial Overlay District

### *Purpose*

The Continuing Light Industrial (CLI) Overlay District intends to allow the continuing use of existing warehouse and light industrial buildings that were developed for these uses before the CE and CAC Districts were adopted. To use the CLI Overlay District, the property must be located in the CE or CAC District and must have either single or multiple tenant warehouse or industrial buildings. Limited new development in the CLI Overlay District is permitted. Any required improvements to the streetscape and site design should enhance the appearance of the corridor.

### *Land Use Goals*

1. Encourage the continuing use and renovation of existing warehouse and industrial facilities.

2. Allow for continuing uses, with limited expansion of buildings and sites. Help bring existing properties into compliance with the Route 1 Manual over time.

### *Design Concepts*

1. Encourage streetscape improvements with required sidewalks and street trees.
2. Promote access improvements and circulation designs that enhance traffic safety and accommodate transit and pedestrian travel.
3. Add landscaping to parking areas and screen any outdoor storage areas.

The photographs in Figure 2.17 show several examples of warehouse and light industrial building types for the CLI Overlay District. To the extent permitted, all additions or improvements to the existing buildings or to the site are encouraged to respond to the land use goals and design concepts of the applicable CE or CAC District.

**Figure 2.17. Building Types for the CLI Overlay District**





## STREETSCAPE DESIGN

***Purpose:** This chapter sets forth requirements and recommendations for the design of the public right-of-way. A well-functioning road network is a key component of the revitalization of the Route 1 corridor. The appearance of the public streetscape is also of paramount importance since most people's perception of the corridor comes from travel on the roadway. Therefore, the requirements and recommendations of this chapter focus on improvements to both the function and the appearance of the public roadways. Because of the increased pedestrian orientation of the new districts, this chapter emphasizes the needs of the pedestrian. The image of the streetscape does not end at the edge of the right-of-way; private improvements along the right-of-way are an essential part of the streetscape and are discussed in subsequent chapters on Site Design and Building Design.*



Figure 3.1. Older sycamores along the roadway edge in Elkridge are the inspiration for the major street tree selected for Route 1.



Figure 3.2. The stone work of the historic Thomas Viaduct is the inspiration for the stone patterns used along Route 1.

### Design Intent

The Route 1 corridor is currently visually chaotic, diverse and not very attractive. The Route 1 Manual identifies design concepts that introduce some continuity and create a style that is distinctive to the corridor and capitalizes on its character. Many of the requirements and recommendations in this chapter are based on streetscape design ideas endorsed in the Route 1 Corridor Revitalization Study, Phase 2 Report. That study established a design theme based on three landscape materials - sycamores, stone and steel - as representative of the corridor's history. Sycamores once lined parts of the corridor. Some remnants remain; thus the smaller, healthier London Plane Tree (a relative of the sycamore) is suggested as a primary street tree (Figure 3.1). The stone of the Thomas Viaduct in Elkridge inspired the gray, stone-like pavers or paving patterns that are recommended for special edging and crosswalks (Figure 3.2). The Bollman Bridge, with its historic industrial ironwork, and other steel structures motivated the selection of an attractive industrial style of street furniture (Figure 3.3). The bridge's red color can be used to enliven elements of the streetscape design.

Using these design elements and materials, the County is moving forward with construction of streetscape improvements and four community gateway signs in the corridor. It is hoped that these streetscape improvements and design ideas will begin to establish a unique visual identity for the corridor and

that future streetscape improvements will continue to foster the recommended design character. Property owners and developers throughout the corridor are therefore encouraged to consider these design ideas when they improve their properties.

## Applicability

In this chapter, the requirements and recommendations for streets and streetscape improvements apply not only to sites in the three districts, but also to all sites that abut the Route 1 right-of-way. These requirements and recommendations apply to both of the following cases:

1. Public street improvements made by the State or County government.
2. Required improvements in the Route 1 design right-of-way and to other streets made by private property owners. Where there is inadequate right-of-way, improvements will be located on private property adjacent to the right-of-way.

For all public street rights-of-way, private roads and access driveways, the Howard County Subdivision and Land Development Regulations and the Design Manual, Volume III, list specific requirements for such elements as sidewalks, street trees, landscaping, access consolidation and driveway connections between parcels. This chapter articulates how these requirements should be met for developments in the three new districts and for all properties adjacent to the Route 1 right-of-way. Where the requirements of the Route 1 Manual exceed or are more restrictive than those of other documents, the requirements of this Manual shall apply. The State Highway Administration (SHA) is not responsible for the installation nor the maintenance of street trees and lighting enhancements required by the Manual. SHA is not responsible for the maintenance of sidewalks within the State right-of-way. Property owners are responsible for maintenance of sidewalks within County or State rights-of-way abutting their property.

## Road Network

### Goals:

The Route 1 corridor should have safe vehicular and pedestrian circulation systems that are separated from



*Figure 3.3. The color and historical industrial design character of the Bollman Bridge is the inspiration for street furniture along Route 1.*



*Figure 3.4. Where space allows, a landscaped median can control traffic movements, increase pedestrian safety and make a more attractive roadway.*

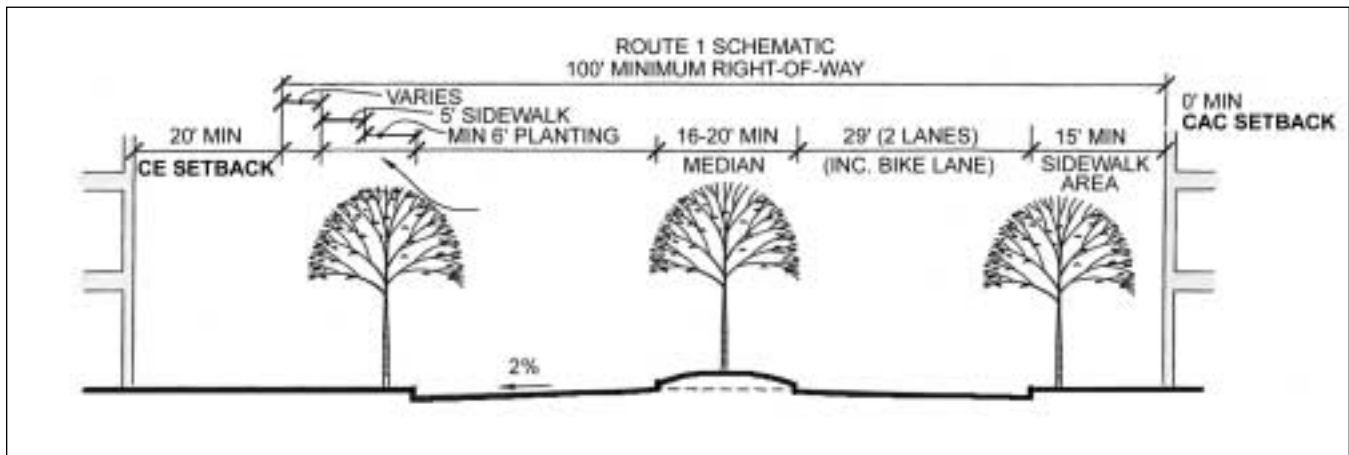


Figure 3.5. This Route 1 schematic cross-section shows the standard 100-foot minimum design right-of-way with a median as it might appear in the CE-CLI and CAC Districts. The right-of-way may need to be increased, especially at CAC Districts and/or where a 20-foot wide median is required for turning movements at unsignalized intersections.

each other. The safety and mobility of through traffic should be promoted by minimizing the number of access points to private property from public streets. Landscaped medians should be provided wherever feasible to enhance the appearance and to provide safe refuge for pedestrians crossing the street (Figure 3.4).

### Requirements:

1. Comply with the road design and construction requirements of the Howard County Subdivision and Land Development Regulations, Sections 16.119 and 16.132.
2. Comply with the Design Manual, Volume III, and its requirements for road construction.
3. Comply with SHA requirements for improvements needed to obtain an access permit.
4. Comply with the Route 1 design right-of-way, 100 feet minimum, or 50 feet minimum from center line, established by the State Highway Administration (SHA) and the Department of Planning and Zoning (DPZ). SHA and the County will determine which of the following configurations to use based on design right-of-way, access, traffic and safety considerations (Figure 3.5):
  - a. Construct a five-lane cross-section with center turning lane.
  - b. Where through movement and access controls are high priorities on Route 1, provide a four-lane cross-section with a median that has limited, controlled access points.
5. If required by SHA, DPZ and/or other County agencies, provide a design right-of-way of 120 feet or greater if the need for additional width is

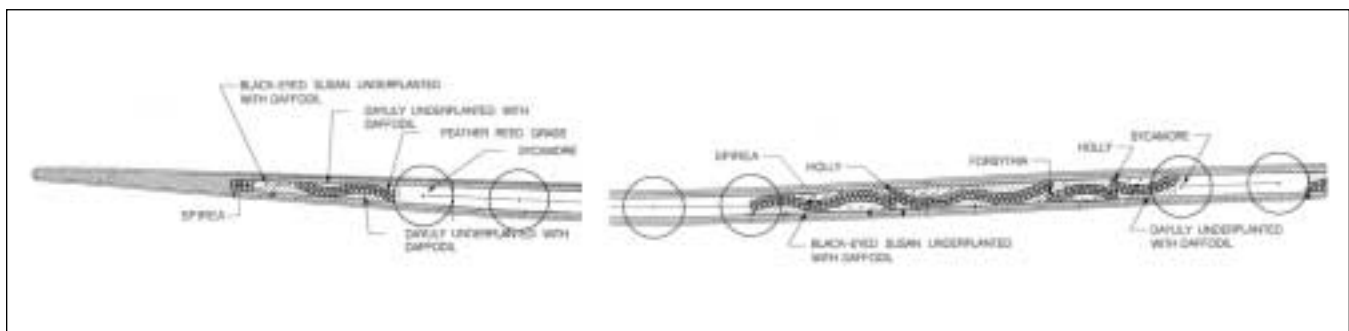


Figure 3.6. Preliminary designs for Route 1 medians show concrete pavers at the ends and along the edges of the median. Planting includes shade trees and massings of shrubs, ornamental grasses and bulbs. This design is subject to change.

anticipated to accommodate acceleration/deceleration lanes, additional through lanes, added turn lanes or improvements to accommodate persons with disabilities, pedestrians, bicycles and/or transit vehicles.

6. Provide five-foot wide bike lanes for northbound and southbound travel along Route 1 as shown Figure 3.5.

### ***Recommendations:***

1. Plant Route 1 medians using the DPZ approved planting concept. The preliminary planting concept (Figure 3.6) is subject to change. See DPZ for the final approved planting concept and recommended plant list.
2. In the TOD and CAC Districts where roadway improvements occur, provide on-street parking, as approved by SHA and/or the Department of Public Works.
3. In all districts, construct interconnecting streets wherever possible and avoid using cul-de-sac streets except where environmentally sensitive features require them.
4. In the TOD and CAC Districts, construct roads that connect neighboring developments.
5. Where retail uses are concentrated, consider designing service roads (Figure 3.7).



*Figure 3.7. Service roads can provide connections between adjacent commercial uses, limiting the need to travel along arterial highways.*

## **Sidewalks and Crosswalks**

### ***Goals:***

The Route 1 corridor should have a continuous street sidewalk system to enhance pedestrian use and safety. Sidewalks along the Route 1 roadway should provide a consistent appearance throughout the corridor by using the same or similar materials and designs.

### ***Requirements:***

1. Comply with sidewalk construction requirements in Section 16.134 of the Subdivision and Land Development Regulations, and the Design Manual, Volume III.



Figure 3.8. 15-foot wide sidewalks provide adequate room for pedestrians and street furniture in TOD and CAC Districts.

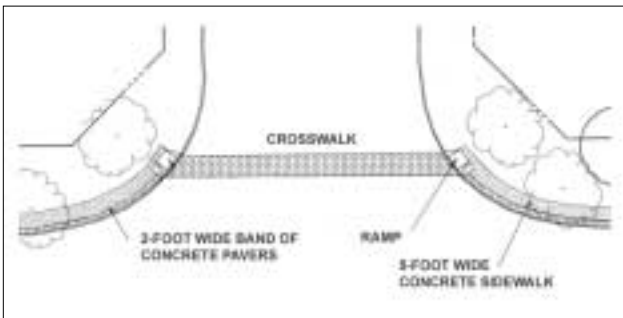


Figure 3.9. This crosswalk detail shows pavement treatment at an intersection to highlight pedestrian crossings.

2. In the CAC District along the Route 1 roadway and in the TOD District along major pedestrian access roads that lead to the MARC station, install sidewalks with a minimum width of 15 feet from face of curb to face of building. These sidewalks may have cutouts for street trees and may accommodate street signs and other such public features (Figure 3.8). The 15-foot dimension shall include a 2-foot wide band of concrete pavers or stamped concrete along the curb edge.
3. In the CE-CLI and CAC-CLI Districts along Route 1 and on side streets within the TOD and CAC Districts, install sidewalks with a minimum width of 5 feet. Where adequate right-of-way exists, a minimum 6-foot wide green strip should be located between the curb or edge of paving and the sidewalk to provide space for planting trees.
4. Provide special crosswalk pavement treatment at intersections to enhance visibility of crossings and to distinguish the pedestrian from the automobile network.
  - a. For public road rights-of-way that intersect with Route 1 where the crosswalk is parallel to and within the Route 1 right-of-way, the approved crosswalk is stamped asphalt (Figure 3.9).
  - b. Crosswalks perpendicular to Route 1 shall have white thermoplastic striping.
  - c. On County rights-of-way alternative materials (concrete pavers, stamped concrete, brick, etc.) must be approved by the Departments of Public Works and Planning and Zoning.
5. In the TOD and CAC Districts, construct sidewalks that connect to neighboring developments.

### ***Recommendations:***

1. Place a 2-foot wide band of concrete pavers between the curb and the sidewalk where sidewalks must be placed against the curb because the right-of-way is insufficient for a 6-foot wide green strip (Figure 3.9). This detail will enhance pedestrian safety, make the right-of-way more attractive and reduce maintenance needs associated with narrow grass strips.

2. Use special paving (stamped concrete or concrete pavers) at pedestrian entrances to multiple building developments. The preferred pattern for the stamped concrete or the concrete pavers should be similar to the stone-like details proposed in the County's streetscape plans for the corridor (Figure 3.10).

## Street Trees

### Goals:

The visual character of the corridor should be enhanced with the use of street trees along all public road rights-of-way, private roads and access driveways. Street trees define the street edge and establish a unifying element for the streetscape. Trees serve several aesthetic functions such as defining edges and spaces, directing views and strengthening vistas. Environmental functions served by trees include providing a shaded canopy for pedestrians, reducing ambient air and surface temperatures, improving air quality and reducing ground-level ozone.

### Requirements:

1. Comply with the requirements for the installation of street trees in Section 16.136 of the Subdivision and Land Development Regulations, and Chapter V of the Landscape Manual. Comply with applicable AASHTO standards for street tree setbacks, clear zones and sight lines per Chapter 2 of Volume III of the Design Manual.
2. Install street trees along both sides of all public roadways, private roads and access driveways in the corridor.
3. In the CE-CLI, TOD and CAC-CLI Districts, use medium to large deciduous trees with ultimate heights above 40 feet. Plant the trees 40 feet maximum on center with a minimum 2.5-inch caliper.
4. Use small to medium trees with an ultimate height of 20-40 feet in areas with overhead obstructions. Plant the trees 30 feet maximum on center with a minimum 2.5-inch caliper.
5. Provide a minimum 6-foot wide planting area for street trees. Where planting areas are located within 15-foot wide sidewalks, the area excavated

#### *Recommended Edging for Sidewalks and Medians*

Concrete pavers, cobblestone pattern, shadow (gray) color, manufactured by Abbotsford Concrete Products, Ltd., or equal by another manufacturer.

#### *Recommended Edging for Islands without Curbs*

Stamped concrete paving, running bond large cobblestone pattern, laid parallel to the paving edge, shale (gray) color, full depth color release, manufactured by Symons Corporation, or equal by another manufacturer.

#### *Recommended Crosswalk Paving*

Stamped asphalt paving, British cobble pattern, granite color, with Texas cobble edges, concrete gray color, manufactured by StreetPrint, or equal by another manufacturer.

#### *Recommended Crosswalk Striping*

White thermaplastic lines, all lines 1-foot wide; outside edges to be 2 parallel lines, 10 feet apart; internal striping to be perpendicular to the edge lines, 2 feet apart.

*Figure 3.10. The preliminary list of recommended paving materials, patterns and colors is subject to change. See the Department of Planning and Zoning for the final approved list and details.*

### ***Recommended Primary Street Tree***

Platanus x acerifolia - London Plane Tree

### ***Recommended Medium to Large Street Trees***

Acer rubrum - Red Maple (varieties)  
Fraxinus americana - White Ash (seedless varieties)  
Fraxinus pennsylvanica lanceolata - Green Ash (seedless varieties)  
Ginkgo biloba - Ginkgo (male only)  
Gleditsia triacanthos inermis - Thornless Honeylocust (varieties)  
Quercus phellos - Willow Oak  
Quercus rubra - Red Oak  
Quercus robur - English Oak (fastigiate form)  
Zelcova serrata - Japanese Zelcova

### ***Recommended Small to Medium Street Trees (under or immediately adjacent to power lines only)***

Acer ginnala - Amur Maple  
Acer campestre - Hedge Maple  
Acer griseum - Paperbark Maple  
Crataegus crusgalli inermis - Thornless Cockspur Horthorn  
Malus species and hybrids - Flowering Crabapple (Adams, Centurion, Harvest Gold, Zumi, Snowdrift, etc.)

Figure 3.11. The recommended trees for Route 1 have been selected for their hardiness, tolerance of urban conditions and form. Other selections may be approved by the Department of Planning and Zoning.

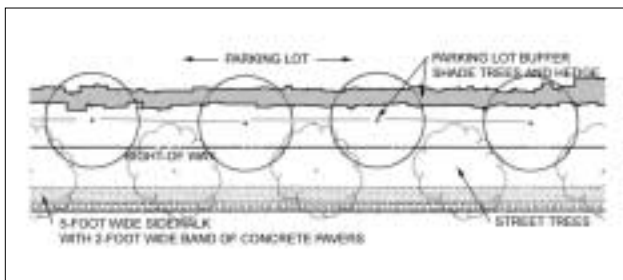


Figure 3.12. An attractive double row of staggered trees can be created along Route 1 when the placement of the required street trees is coordinated with the placement of the required on-site landscape buffer trees.

for the planting pit should be 6 feet wide, 8 feet long and 4 feet deep.

### ***Recommendations:***

1. Use the tree palette selected for the Route 1 roadway to provide consistency and to establish a distinct identity for the major roads in the corridor. Initial tree plantings by the County will be Platanus x acerifolia, London Plane Tree, however, development of a tree monoculture (use of only a single species of tree) should be avoided. Figure 3.11 lists principal trees recommended for the Route 1 right-of-way and the CE-CLI, TOD and CAC-CLI Districts. The Department of Planning and Zoning may approve other trees listed in the Landscape Manual. Native trees species are strongly recommended. Invasive exotic trees are prohibited.
2. Choose plants that are suitable as street trees based on the following factors: overhead utility lines, shape of canopy, sun and shade tolerance, drought tolerance, maintenance requirements and tolerance of adverse urban conditions.
3. To provide a more formal or urban character along the Route 1 roadway, plant street trees parallel to the applicable required on-site landscape buffer trees (in the setback from the design right-of-way) to provide a staggered double row of trees (Figure 3.12).

## **Street Furniture and Amenity Areas**

### ***Goals:***

Street furniture, as well as street trees, can help provide continuity between project sites and help support pedestrian travel along streets and pedestrian connections to transit areas. Amenity areas in the form of open areas for the public can provide places to gather. They can also provide a distinct identity, a sense of orientation and a visual focus.

### ***Recommendations:***

1. Provide trash receptacles, benches, bicycle storage and gathering areas to serve the needs of pedestrians, and encourage walking and transit use.

Recommended street furniture or equal, described below and in Figures 3.13 through 3.16, should be black or red powder-coated metal, as approved by DPZ.

2. Provide space for a bus stop or bus shelter, if required by the Department of Planning and Zoning. This includes the potential requirement for parallel bus loading areas or pull-outs. Use the approved design, or comparable design, for bus shelters along the Route 1 right-of-way (Figure 3.13). The County or its designated agent will be responsible for the installation.
3. Locate bus shelters close to existing and expected bus patronage generators such as retail centers, office and commercial uses, multi-family housing, institutional uses and major employment centers, and also at bus transfer points. The Department of Planning and Zoning's Division of Transportation Planning maintains a map showing existing and recommended bus stop locations.
4. Use the recommended trash receptacle, or equal, along the Route 1 right-of-way (Figure 3.14). Locate trash receptacles in pedestrian activity areas in the TOD and CAC Districts, and at bus stops and shelters.
5. Use the recommended bench, or equal, along the Route 1 right-of way (Figure 3.15).
6. Use the recommended bollard, or equal, along the Route 1 right-of way (Figure 3.16). Bollards may be useful for those areas where pedestrian traffic needs to be shielded from possible vehicular traffic. Bollards must be set back a reasonable distance from the right-of-way so they do not pose a hazard to vehicular traffic. The recommended bollard also functions for bicycle storage.
7. Locate bicycle lockers and/or racks near MARC transit stops in highly visible and well-lit areas.
8. In the CE-CLI, TOD and CAC-CLI Districts, take advantage of the placement of buildings and walls that create opportunities for plazas, courtyards or garden/patio areas. Such areas can serve as gathering places for pedestrians and as common areas for employees. Provide seating and lighting for outdoor gathering areas.



*Figure 3.13. The recommended bus shelter for the Route 1 corridor is Slimline, 5-foot by 10-foot, black or red powder-coated metal, manufactured by Brasco International, Inc., or equal by another manufacturer.*



*Figure 3.14. The recommended trash receptacle for the Route 1 corridor is Ironsites series model S-42, 36-gallon capacity, manufactured by Victor Stanley, Inc., or equal by another manufacturer.*





Figure 3.15. The recommended bench for the Route 1 corridor is Classic series, model cs-138, 6-foot length with center arm rest, manufactured by Victor Stanley, Inc., or equal by another manufacturer.



Figure 3.16. The recommended bollard for the Route 1 corridor is Bollard 35, BKR permanent mount, manufactured by Victor Stanley, Inc., or equal by another manufacturer. The bollard can also serve as a bicycle storage area.

9. Sidewalk dining can enhance an urban atmosphere and increase activity on the street. Outdoor dining areas should not encroach on the required minimum dimensions for sidewalks.

## Pedestrian Street Lights

### Goals:

Ornamental pedestrian street lights help create a distinct identity along the corridor and help promote pedestrian safety. The use of these fixtures is especially important in the pedestrian-oriented TOD and CAC Districts, but it can also be beneficial in areas of high pedestrian use such as major intersections where retail uses or high-density housing occur.

### Requirements:

1. Comply with the requirements in the Outdoor Lighting section of the Zoning Regulations and Section 16.135 of the Subdivision and Land Development Regulations.
2. In the TOD and CAC Districts, provide pedestrian light fixtures. Use the recommended teardrop fixture, or equal, along the Route 1 right-of-way or along the main pedestrian routes to the MARC stations where minimum 15-foot wide sidewalks are required (Figure 3.17). Spacing of fixtures will be determined by engineering analysis.

### Recommendation:

1. Provide pedestrian light fixtures. Use the recommended fixtures in the TOD and CAC Districts and elsewhere in the corridor, particularly where concentrated pedestrian activity occurs.

## Utilities

### Goals:

The visual and noise impacts of utilities, mechanical equipment, data transmission dishes, towers, and similar antennas and equipment should be minimized by their placement, or mitigated with planting or architectural features.

### ***Requirements:***

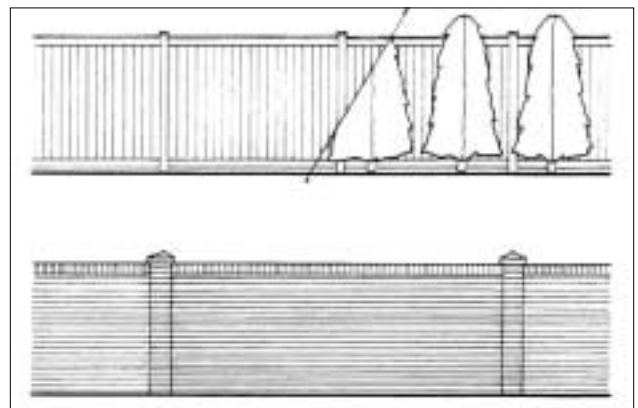
1. Locate transformers, telecommunications devices, equipment switching boxes and other utility cabinets away from the street, major pedestrian routes and outdoor seating areas. Locate equipment and devices away from residential areas, especially if they produce noise.
2. If transformers, telecommunications devices, equipment switching boxes and other utility cabinets must be located in an area visible from the street and pedestrian areas, then buffer with architectural fence, wall, and/or landscaping screens (Figure 3.18).

### ***Recommendations:***

1. Install all permanent utility lines underground, where feasible. Move utilities to the rear of the property to help hide them along the roadway. Use dark metal poles for utility lines in place of wood poles.
2. Install underground conduits where feasible, so that utilities can be moved underground in the future if cost and construction improvements allow.



*Figure 3.17. The recommended ornamental street light is black finish, teardrop fixture SB470A with single arm adapter SA8470, manufactured by Hadco, or equal by another manufacturer. The recommended pole is tapered, fluted black metal with wrap-around base. The recommended light is subject to change. See DPZ for the final approved light and pole.*



*Figure 3.18. Examples of continuous, solid, opaque screening fences or walls with landscaping.*

## SITE DESIGN

***Purpose:** The Route 1 corridor's three new zoning districts are intended to encourage new land development patterns. Most of the existing patterns are suburban in character and use large amounts of land at relatively low intensities. The new patterns intend to make more efficient use of the land and to establish a more urban character for portions of the corridor. This chapter provides guidance in achieving the new patterns.*



Figure 4.1. Buildings that line the right-of-way establish a uniform street edge and provide an inviting scale for pedestrians.

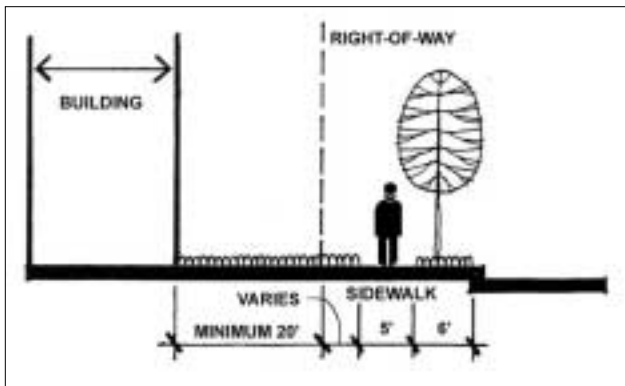


Figure 4.2. CE setback and building placement will make buildings more prominent by reducing the distance from the edge of the road.

### Building Location

#### Goals:

The location of buildings should reinforce the street as the center of activity and take advantage of the Route 1 streetscape improvements, particularly in the CAC District. Setbacks from the street are generally experienced as part of the public realm. To produce a more urban character, the buildings should be close to the sidewalks and rights-of-way (Figure 4.1). The buildings should extend across a substantial portion of the property's frontage so that a more urban and pedestrian friendly environment can be created. Buildings should be clustered to establish a uniform street edge thereby enhancing the visual quality of the streetscape.

#### Requirements:

1. In the CE District, place buildings at a 20-foot setback from the Route 1 design right-of-way (Figure 4.2).
2. In the CE District, buildings shall occupy preferably 50% but not less than 30% of a property's Route 1 right-of-way frontage (Figure 4.3).
3. In the TOD District, place buildings at the zero setback along roads that serve as major pedestrian access routes to MARC stations (Figure 4.4).
4. In the TOD and CAC Districts, buildings may be set back a maximum of 10 feet from the design right-of-way to provide variety in the streetscape and architectural treatment. This setback may be used to accommodate amenity plantings, outdoor seating, courtyards, plazas, entries, outdoor

eating areas, displays or other special features. In the TOD District, a 10-foot setback may also be used to meet the added setback requirements for building heights over 60 feet (see Chapter 5, Building Design).

5. In the TOD or CAC Districts, buildings shall occupy at least 75% of a property's frontage along major pedestrian roads to MARC stations or of a property's Route 1 right-of-way frontage, respectively (Figure 4.5). This will create a more continuous pedestrian friendly environment.
6. In the CAC District, place buildings at a zero setback to the Route 1 design right-of-way.

### Recommendations:

1. Throughout the corridor, locate the office portion of flex-space and light manufacturing uses in the front of buildings, facing the street.
2. Unify various site components, including buildings, accessory structures, fencing, walls, signage, etc. through the use of similar design, materials and colors.
3. Establish a visual or physical link among various buildings in multi-building complexes by using site design elements such as courtyards, plazas, landscaping and walkways to unify the project.
4. Where there are two or more office buildings located on a site, orient buildings toward public streets. Provide a view of the building entrances from the street, possibly across a green space or through a plaza (Figure 4.6).

## Vehicular Access

### Goals:

Safe vehicular access is always a concern at site entrances; however, especially in the TOD and CAC Districts where pedestrian traffic is encouraged, pedestrian traffic safety is also a major concern. To increase pedestrian safety, vehicular traffic patterns need to be separated from pedestrian traffic patterns. For safety, properties in the Route 1 corridor should be designed to limit the number of curb cuts, consolidate entrances and articulate them for ease of identity, and ensure that pedestrian crossings are clearly marked. Curb

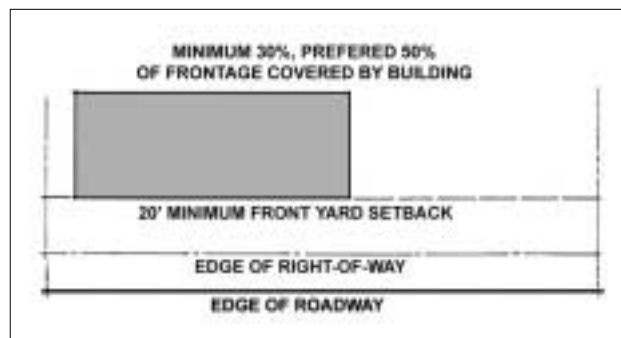


Figure 4.3. In the CE District, buildings should span a significant portion of the Route 1 right-of-way frontage.

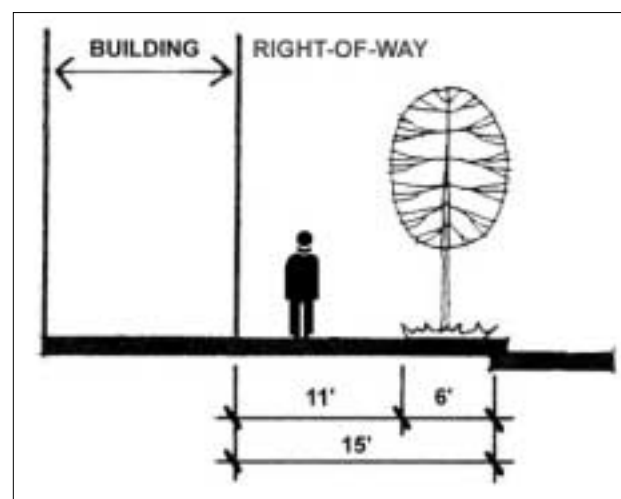


Figure 4.4. TOD and CAC setbacks and building placement will create an attractive area for pedestrians.

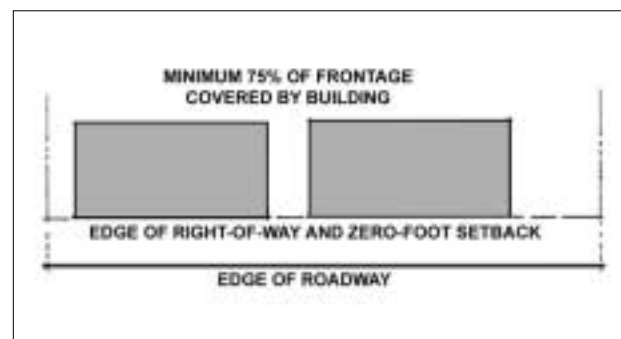


Figure 4.5. In the TOD and CAC Districts, buildings should dominate the Route 1 right-of-way frontage to produce a more urban and pedestrian friendly environment.

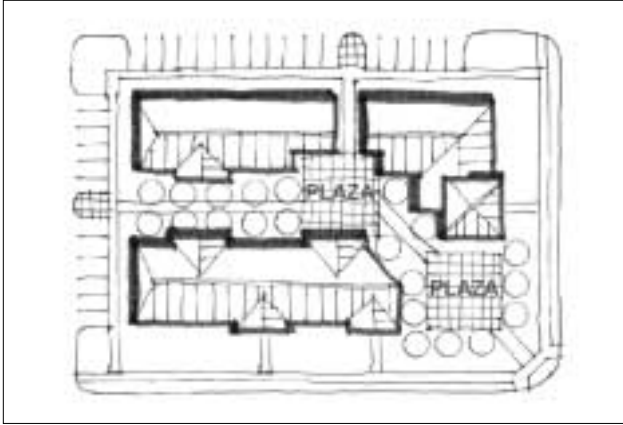


Figure 4.6. Office buildings oriented to street and to plaza or green space become important elements of the streetscape.

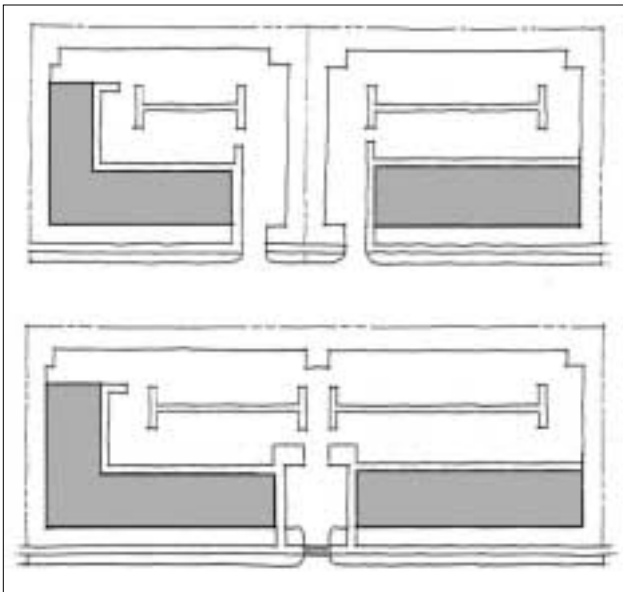


Figure 4.7. A minimum number of curb cuts reduces traffic congestion, improves safety and allows for additional parking.

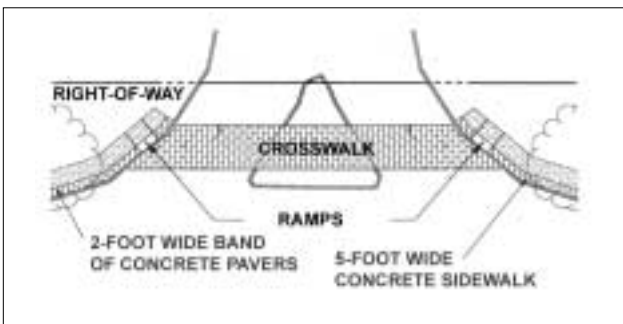


Figure 4.8. Crosswalks across driveway entrances to major retail centers and employment complexes help to highlight pedestrian crossings.

cuts tend to increase pedestrian exposure to moving vehicles, limit opportunities for landscaping, street trees and on-street parking spaces, and reduce the sidewalk space available for pedestrian interaction and amenities.

### Requirements:

1. Comply with the access requirements in Section 16.119(b)(4) of the Subdivision and Land Development Regulations, and the Design Manual, Volume III.
2. Use the minimum number and size of curb cuts to promote traffic safety, to minimize the pedestrian/vehicle conflict and to maximize building frontage (Figure 4.7).
3. Articulate site entrances through building placement, landscaping features, signage, low walls or similar structures to promote safety by making the entrances visually clear.
4. Continue the sidewalk pattern across driveways that serve as main entrances to major retail and employment centers (Figure 4.8). The pattern shall be thermoplastic white striped markings, at the minimum. Alternatively, use the approved stamped asphalt crosswalk pattern (see Chapter 3, Streetscape Design, Figure 3.9).

### Recommendations:

1. Share entrances with adjacent parcels. Shared access reduces traffic congestion (and also conflicts between pedestrians and vehicles) by consolidating entrance and exit points, and by allowing vehicular movement between properties without using the street. Especially in the CAC District, shared entrances help to achieve the 75% building frontage of properties on the Route 1 roadway.
2. In the TOD and CAC Districts, evaluate using one-way vehicular access to properties to limit the driveway width, thus reducing the distance that pedestrians must cross (although the number of curb cuts may be increased).
3. Use minimum, rather than maximum, commercial driveway widths stipulated in the Design Manual,

provided a turning movements analysis supports the minimum.

## Parking Areas

### Goals:

Convenient parking is necessary but the visual impact from the corridor's public street rights-of-way of parked cars in parking lots should be reduced. The locations of parking areas should be restricted and plantings that help screen the cars from view should be provided. Because prominent parking areas deaden pedestrian activity, they should be located away from pedestrian centers of activity, especially in the pedestrian-oriented TOD and CAC Districts.

### Requirements:

1. In the CE, TOD and CAC Districts, locate parking areas at the side or rear of buildings (Figure 4.9).
2. In the CE District, minimize the number of parking spaces located between the primary façade of the building and the street right-of-way. The maximum building setback of 100 feet allows for a single-loaded bay of parking between the building and the design right-of-way.
3. Create landscaped islands and medians that divide the parking areas and that help define traffic patterns.
4. Provide curbs to keep vehicles from damaging buildings and landscaping. Allow wheel stops at parking edges where landscaped areas serve as bioretention or infiltration areas for stormwater management.
5. For safe pedestrian access to buildings, provide walkways and paths from parking lots to building entrances.
6. Provide landscaped areas between all parking areas and buildings, except for loading/parking areas on the rear of flex space buildings and for loading areas in general. Increase the size of these landscaped spaces in proportion with the increased height of the building.
7. Comply with the requirements in Chapter IV of the Landscape Manual and with the requirements of

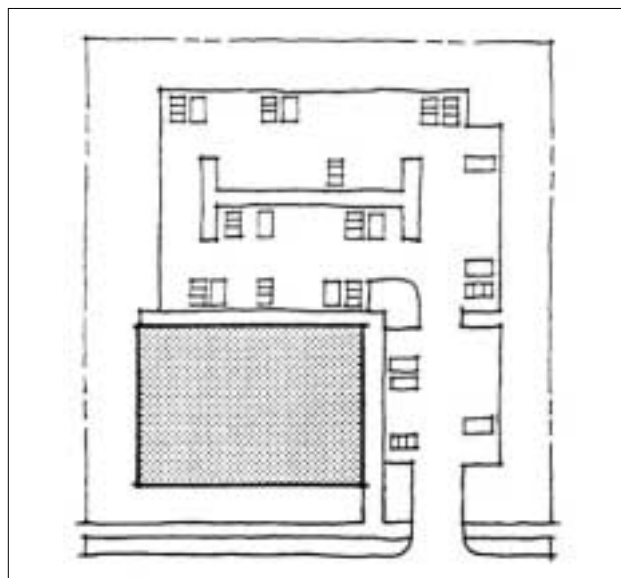


Figure 4.9. Parking areas should be located at the side and rear of buildings.

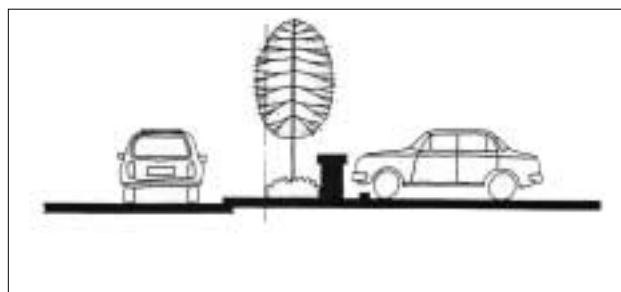


Figure 4.10. Walls and plantings along parking lots at street edge help reduce the visual impact of parking lots from the street.

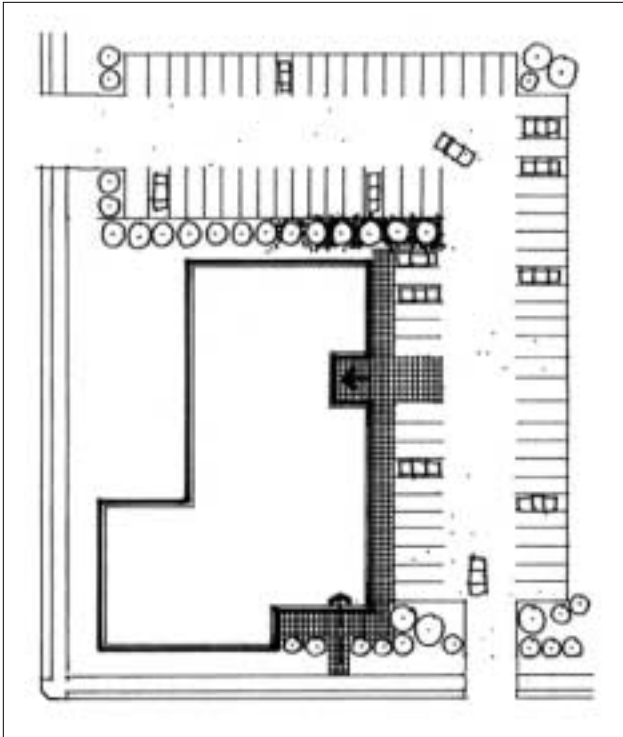


Figure 4.11. Drop-off areas and pedestrian areas with special paving and plantings make building entrances more attractive and easier to identify and access.

this Manual for landscape buffers between parking lots and public streets. Where the requirements of this Manual exceed or are more restrictive than those of the Landscape Manual, the requirements of this Manual shall apply (see Landscape Planting & Screening, below).

### **Recommendations:**

1. Without limiting sight distance, add architectural elements such as low walls or fences at the main entrance to a parking lot to accent the entry and to create a sense of street edge (Figure 4.10).
2. Link parking lots for adjacent commercial properties by a common drive aisle. The intent is not that parking be shared but to provide ease of access, limit curb cuts along major roads and reduce the number of trips that use major roads. Drive aisles may occur in various locations, most commonly in front of buildings as a service road or at the rear of properties or other locations.
3. Do not locate parking directly in front of main entrances to buildings. Provide drop-off areas or expanded pedestrian areas with enhanced paving and plantings (Figure 4.11).

## **Loading and Storage Areas**

### **Goals:**

In all parts of the Route 1 corridor, to provide an attractive image of properties, loading and storage areas should be located away from and screened from public view. A safe environment should be created by minimizing conflicts between loading areas and travelways.

### **Requirements:**

1. Prohibit any loading at the front of a building or between the building and any adjacent public right-of-way. Locate loading docks, outside storage and service areas away from public view, at the rear (non-street side) of buildings. If the site is constrained, these areas, if screened, may be on the side of buildings.
2. Comply with the requirements in Chapter IV of the Landscape Manual and with the requirements of

this Manual for landscape buffers and screening of loading and storage areas. Where the requirements of this Manual exceed or are more restrictive than those of the Landscape Manual, the requirements of this Manual shall apply (see Landscape Planting & Screening, below).

### Recommendations:

1. Combine service areas or locate them next to each other in multi-building complexes to minimize the visual and noise impacts on the surroundings.
2. Locate loading areas in a service yard at the rear of a multi-building complex, preferably incorporated into the design of the building (Figure 4.12).
3. Minimize the interaction between service vehicles and parked automobiles in service yards and loading areas. Where possible, separate parking areas from service yards and loading areas.

## Landscape Planting and Screening

### Goals:

Planting and screening should be used to improve the appearance of properties along Route 1, hiding unattractive views and buffering incompatible uses. These refinements and expansions of the requirements of the Landscape Manual are intended to establish a distinct landscape character for the Route 1 roadway. Formal landscape treatments can reinforce the desired image of a more urban character for the corridor. In the three new districts, where setbacks are reduced and development intensity may increase, planting can help soften and beautify the environment.

### Requirements:

1. Within the CE, TOD and CAC Districts, provide a Type D screen along the perimeter of all non-residential uses that adjoin residential properties (Figure 4.13). Non-residential uses include buildings, parking, loading and landscape areas, but do not include retained forest protected by forest conservation easements. The increased planting

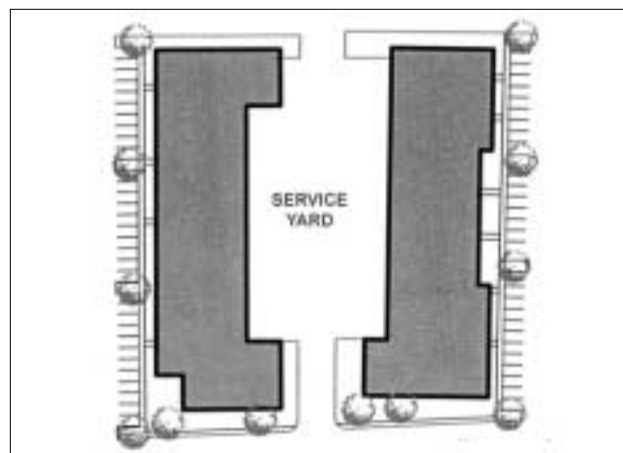
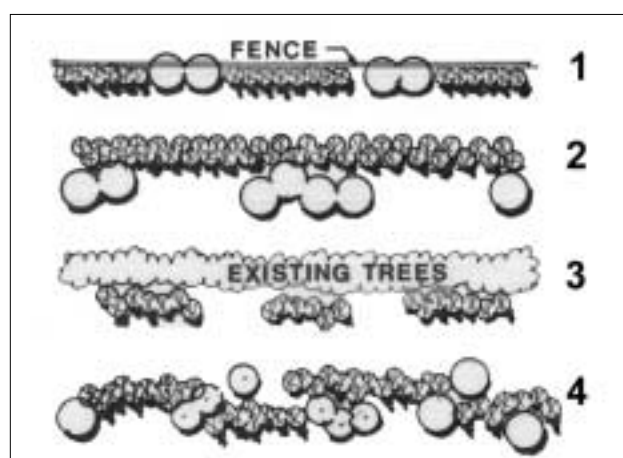
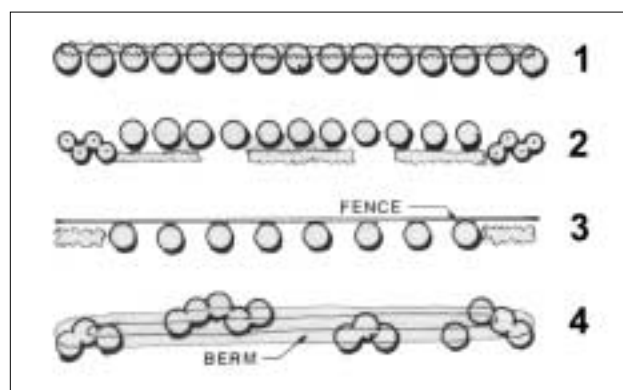


Figure 4.12. Shared loading areas can be located away from public view.



4.13. Recommended Type D perimeter screens: options 1, 2 and 3 are preferred for their formal planting layout; option 4, with its informal massings, is not desirable.



4.14. Recommended Type E perimeter buffer: options 1, 2 and 3 are preferred for their formal planting and fence or wall; option 4, with berm and informal massings, is not acceptable.





Figure 4.15. Exposed storage areas along Route 1 enclosed with chain link fence are unsightly.



Figure 4.16. Storage yard and chain link fence can be efficiently screened with a Type D buffer. Plantings must comply with applicable setbacks, clear zones and sight lines in the Landscape Manual and per AASHTO standards in Chapter 2 of Volume III of the Design Manual.

requirements are intended to mitigate reduced setbacks allowed in these districts.

2. In the CE, TOD and CAC Districts, screen parking lots from the street using a Type E buffer; however, the berm option is excluded. To ensure that planting patterns will be consistent along Route 1 frontage, DPZ may require the Type E buffer to be extended across the entire frontage, buffering non-residential open space as well as the parking lot (Figure 4.14).
3. Distribute landscaping throughout parking lots to reduce the effect of heat and glare from pavement.
4. Screen all service areas such as loading, trash enclosures and outside storage, using a combination of fences, walls, gates and landscaping. Integrate walls and fences into the landscape design. Fully screen outside storage areas with screen wall or fence enclosures. Stored materials may not be stacked or be visible above the enclosure height.
5. In the TOD and CAC Districts, do not use corrugated metal, corrugated fiberglass, sheet metal or wire mesh as a screening material along the Route 1 frontage. If chain link fencing is used in the TOD and CAC Districts, it must be landscaped with a Type D screen.
6. In all other Districts, if chain link, corrugated metal, corrugated fiberglass, sheet metal or wire mesh fences are needed for security purposes and if they are visible from Route 1, landscaping shall obscure the fence with a Type D screen (Figures 4.15 and 4.16).

### ***Recommendations:***

1. Use walls, fences and plantings to define boundaries, provide access control and distinguish private and public areas.
2. Use formal design layouts for plantings adjacent to the Route 1 right-of-way and in the new zoning districts. Formal plantings are in keeping with the more urban image that is envisioned for the corridor (Figure 4.17).
3. In the CE, M-1, M-2, B-1 and B-2 Districts, discourage the use of chain link, corrugated metal, corrugated fiberglass, sheet metal or wire mesh fences.

4. In all districts, encourage the use of steel picket or solid board, continuous, opaque fencing and/or landscape screening (Figure 4.18).

## Trash Enclosures

### Goals:

Trash enclosures should be located so they are not visible from public rights-of-way, building entrances and amenity areas, and they should be easily accessible by collection trucks.

### Requirements:

1. Enclose dumpsters with a continuous, solid, opaque wall or fence, or a Type D landscape screening treatment.
2. Buildings shall consolidate their trash collection needs in a single, central location away from public view.

## Mechanical Equipment

### Goals:

The visual and noise impacts of utilities, mechanical equipment, data transmission dishes, towers, and similar antennas and equipment should be mitigated.

### Requirements:

1. Screen mechanical equipment on the ground from view of residential developments using a continuous, solid, opaque wall or fence. Select a screen design that is compatible with the architecture of the building(s) on the site.
2. Do not locate mechanical equipment in the setback between the building and the public right-of-way. If it is not feasible to locate the equipment elsewhere, screen the mechanical equipment with a continuous, solid, opaque wall or fence that is architecturally integrated with the building.
3. If allowed by DPZ as part of an integrated landscape design, use a dense landscape screen of evergreen trees or tall shrubs as an alternative to a wall or fence if space is available and if it will not



Figure 4.17. This formal parking lot buffer planting features a row of evenly spaced shade trees and straight hedges. It defines a strong edge for the street, buffers the parking lots, yet does not obscure visibility into the site.

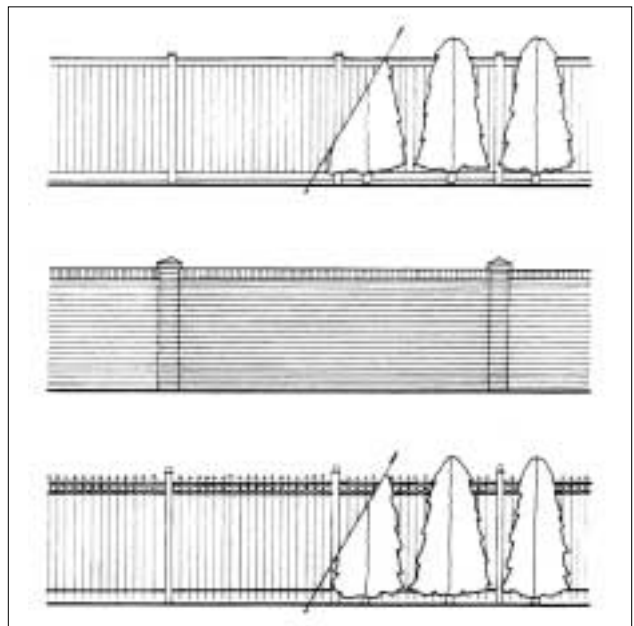


Figure 4.18. Examples of preferred screening walls and fences for loading docks, storage yards, trash enclosures and mechanical equipment include solid board fence, brick wall and steel picket fence.



Figure 4.19. Stormwater management facilities can be visual amenities.

hinder the operation of mechanical equipment. If perimeter site landscape buffers already screen the mechanical equipment, DPZ may waive the requirement for additional screening.

### ***Recommendations:***

1. Architecturally integrate the method of screening with the structure in terms of materials, color, shape and size. Where freestanding mechanical equipment is provided, use a continuous solid screen.

## **Stormwater Management**

### ***Goals:***

Stormwater management in the Route 1 corridor should protect the natural stream system by improving water quality and reducing the volume and velocity of stormwater entering streams.

### ***Requirements:***

1. Comply with the requirements in the Howard County Design Manual, Volume I, and the 2000 Maryland Stormwater Management Design Manual, Volumes I and II.
2. Integrate stormwater management facilities with other site elements through the use of landscaping.

### ***Recommendations:***

1. Utilize multistory buildings to reduce the building footprint and reduce impervious areas. Where appropriate, use parking lot islands and buffers for bioretention and infiltration.
2. Retrofit existing stormwater management facilities to the maximum extent possible.
3. Consider using underground retention facilities where space is limited.
4. Design stormwater management facilities as amenities that are visible from a building or a street (Figure 4.19).
5. Use green or vegetated roof technology to collect and detain runoff.

6. Explore other methods of stormwater management as listed in the 2000 Maryland Stormwater Management Design Manual, Volumes I and II.

## Lighting

### *Goals:*

Exterior lighting should be integrated with the site design so that maximum efficiency of the necessary lighting is achieved, and possible glare and spillover effects of such lighting are avoided.

### *Requirements:*

1. Comply with the Outdoor Lighting section of the Zoning Regulations.

### *Recommendations:*

1. Incorporate exterior light fixture design into the building design and landscape scheme of the development.

## Freestanding Signs

### *Goals:*

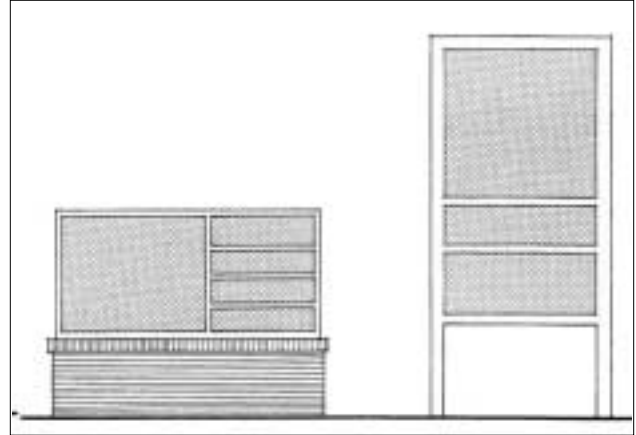
The Route 1 corridor, being an older commercial and industrial corridor, has many freestanding signs that have, over time, helped to create an overall chaotic appearance of the corridor. Citizens have stated that the existing signage along the Route 1 roadway contributes to the corridor's overall negative character. Providing a more consistent placement and orientation of signage should reduce this sense of visual clutter.

### *Requirements:*

1. Comply with the requirements of the Howard County Sign Code.

### *Recommendations:*

1. Instead of freestanding signs, select building mounted or wall signs that are integrated into the building's architecture.



*Figure 4.20. Low-profile signs are preferred over pylon signs.*



*Figure 4.21. Signs that use colors, materials and details derived from the site's architectural features provide an attractive and harmonious image from the street.*

2. For wall signs, please see the chapter on Building Design and the section entitled Signs Attached to Buildings.
3. Use durable materials, subdued colors and professionally executed graphic design.
4. Select monument or low-profile, ground mounted type signs instead of freestanding pole mounted signs (Figure 4.20).
5. Without limiting sight distance, place freestanding signs closer to the property line to promote a street edge appearance.
6. Coordinate materials and colors for freestanding signs with materials and design elements/character from the principal buildings on the site. This helps bring the identity of the building to the street edge (Figure 4.21).
7. Eliminate any glare visible to motorists and pedestrians from the sign's lighting source. Freestanding signs may be internally or externally lit.

## On-Site Pedestrian Circulation

### Goals:

Properties in the Route 1 corridor should offer safe opportunities for walking. A continuous system of sidewalks and crosswalks should be created to connect buildings to sidewalks along public street rights-of-way, link multiple buildings on sites and make connections to amenity areas. This system should provide a comfortable and inviting environment.

### Requirements:

1. Make internal pedestrian walkways a minimum of 5 feet wide.
2. Provide clearly defined paved pedestrian connections:
  - a. Between a public right-of-way and building entrances.
  - b. Between parking lots and building entrances.
  - c. Between transit stops and building entrances.
  - d. Along the front facade of a building with multiple entrances for multiple tenants so that ac-

cess is provided among entrances. This is especially important for retail and flex space buildings. In multi-building complexes, design pedestrian circulation and internal landscape areas so they connect buildings together.

3. Clearly separate pedestrian connections from vehicles by one of the following (Figure 4.22):
  - a. A 6-inch vertical concrete curb.
  - b. Textured paving if pedestrians will share vehicular circulation, particularly in arrival/drop off areas, or where pedestrians cross vehicular lanes.
  - c. A continuous landscape area a minimum 3 feet wide on at least one side of the walkway.

## Site Amenities and Features

### Goals:

Plazas, courtyards or similar visitor and employee amenities should be incorporated into site development plans for office, residential and retail uses, especially if part of a multi-building complex. These areas should be easily accessible and comfortable for a substantial part of the year. In sufficiently large developments, a centrally located public open area or focal point should be linked with other amenities through open area corridors that have paths.

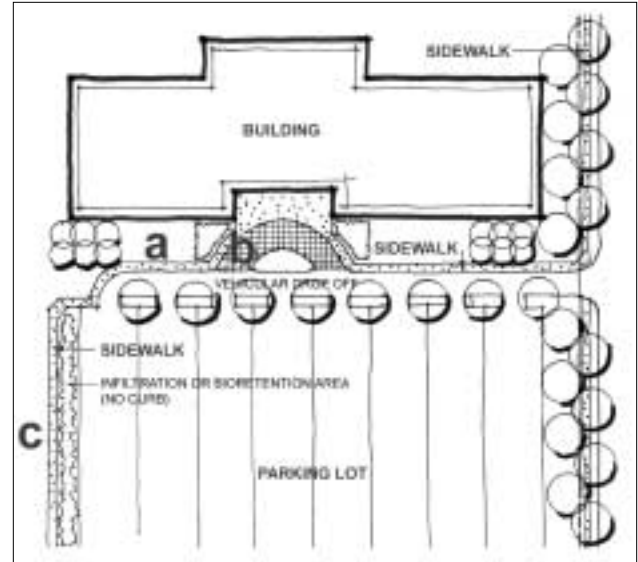
### Requirements:

1. Provide safe, comfortable places where people can stop, view, socialize and rest within the pedestrian circulation system but without impeding pedestrian traffic (Figure 4.23). In the CE District, for sites that are a minimum of 5 acres (not including manufacturing uses), provide 5% of the net site area for pedestrian amenity areas. In the TOD and CAC Districts, provide 10% of the net site area for pedestrian amenity areas.
2. In the TOD and CAC Districts, furnish spaces with pedestrian amenities such as benches, planters, lighting, newspaper racks, transit shelters, landscaping, and/or recreational facilities. Incorporate the site furniture and fixtures into the site design.

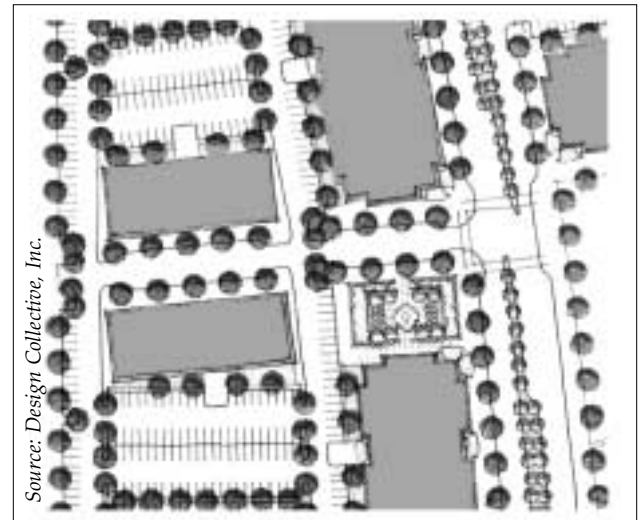
3. Plan courtyard, plazas and other outdoor areas so that they have good physical and visual connections with the building(s).

### ***Recommendations:***

1. Refer to Chapter 3, Streetscape Design for recommended site furniture.
2. Have green space fulfill more than one function. For example, stormwater management facilities may be designed as an amenity or a visual focal point.
3. Orient open area locations with seating that receives both sun and shade.
4. In multi-building complexes, design internal landscape areas that connect the buildings. Accent the walks and open areas with landscaping that provides seasonal interest and color. Take maximum advantage of landscaping by providing outdoor eating areas and plazas between buildings.
5. Where appropriate, provide space for installing bicycle racks or bicycle parking spaces at a minimum of 1 space per 50,000 square feet of gross floor area in all new retail and office developments.



*Figure 4.22. Pedestrian paths should be (a) separated from driveways by a 6-inch curb, (b) clearly defined by textured paving if pedestrians and vehicles share the same routes, and/or (c) separated from vehicular areas by a landscaped strip if no curb or no special change in paving is used.*



*Figure 4.23. Small parks and plazas can be located along streets or within multi-building complexes to provide spaces for gathering and to serve as an attractive focal point.*

## BUILDING DESIGN

***Purpose:** The new zoning districts aim to promote new development patterns that allow for more intensive and efficient use of prime sites. The multistory and greater pedestrian orientation of these new corridor zoning districts is a departure from the current pattern found in business and manufacturing districts. Consequently, the buildings in these locations should have more urban character than buildings in the corridor do presently. The building design requirements and recommendations in this chapter are meant to aid property owners and developers in understanding and achieving the land use goals and design concepts that were agreed upon by citizens and policy makers during the Route 1 Corridor Revitalization Study.*



Figure 5.1. Building facades that have variety and visual interest, especially on the first floor, are important in pedestrian-oriented areas.



Figure 5.2. Although one-story flex space and light industrial buildings will continue in the corridor, multistory office buildings are encouraged in the three new zoning districts.

### Design Concepts

Although this chapter deals with building design, it is not the intent of this Manual to dictate architectural design. Instead, these requirements and recommendations address basic urban/suburban design concepts that relate primarily to land use and site relationships. Many of these requirements and recommendations reflect community opinions expressed in the Visual Preference Survey taken at the citizen workshop.

Because of the concern about the corridor's appearance and the establishment of pedestrian-oriented areas, increased attention should be paid to the visual interest of a building (Figure 5.1). Both for commercial and light industrial buildings, and particularly for those with Route 1 frontage, a well-designed exterior can contribute greatly to improving the appearance of the corridor.

Multistory buildings have traditionally been used to house many different uses in a compact area. Taller buildings can make more efficient use of land and increase the intensity of development, providing the potential for more jobs and more residents in a community (Figure 5.2). Multistory buildings also give a stronger presence on the street than one-story buildings.

To ensure that development complies with this Manual, applicants are required to include schematic architectural elevations on subdivision or site development plans, as applicable, illustrating how building design

responds to this Manual's requirements and recommendations.

## Height

### Goals:

Multistory buildings help form an edge to the street, thus defining public space and differentiating it from private space. The increased density of multistory buildings should yield more potential users for pedestrian and transit improvements. In the new corridor zoning districts, certain ground floor retail and commercial uses are allowed as incentives to encourage the construction of multistory buildings.

### Requirements:

1. In the CAC District, establish a minimum height of 25 feet so that two-story buildings will be constructed (Figure 5.3). Alternatively, use steep roof lines, false fronts, clerestory windows or other such architectural devices to give the appearance of two-story height.
2. In the CE and TOD Districts, use the height provision that allows for buildings over 60 feet in height if setbacks are increased (Figure 5.4). If the building height is greater than 60 feet and the required setback from the right-of-way is zero feet, the following additional setbacks shall be provided:
  - a. Set the portion of the building that is higher than 60 feet back from the street design right-of-way 1 foot for every 2 feet of height above 60 feet, or
  - b. Set the entire building back from the street design right-of-way 1 foot for every 2 feet of height above 60 feet.

## Mass and Articulation

### Goals:

The three new zoning districts in the Route 1 corridor encourage new buildings to be located nearer to the street to help form a more urban character to the corridor. In addition to its location, the building itself can add to the urban character. A building's proportion, height and width, as well as the relationship of various



Figure 5.3. Two-story minimum building heights will improve the scale of buildings and provide opportunities for a mix of uses on upper floors in the CAC District.

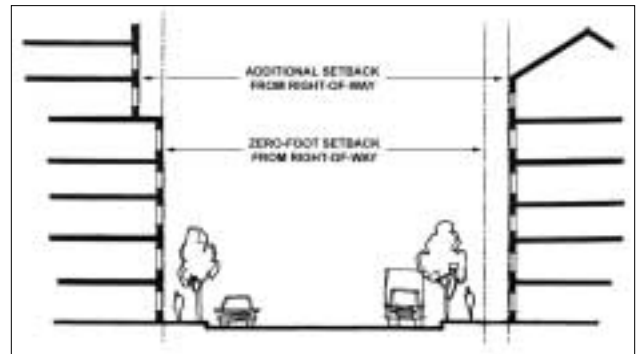


Figure 5.4. To reduce the impact of tall buildings along pedestrian-oriented streets, either the portion of the building that is higher than 60 feet or the building itself must be set back from the street right-of-way.



Figure 5.5. Facades should be divided into 3 parts: base, middle and top. Architectural details are important at the base.





*Figure 5.6. To avoid a large undifferentiated facade, provide articulation through changes in the planes, materials and colors.*



*Figure 5.7. Well-designed corner entry architecture engages the pedestrians and adds visual interest.*



*Figure 5.8. Because of the prominence of corners of buildings, use designs such as towers that emphasize their importance.*

design elements, such as windows and doors, to the entire building should be carefully designed and balanced. A building's size and proportion in relation to other surrounding buildings should be evaluated to ensure a sense of harmony. A comprehensive architectural concept should be developed and maintained in multi-building complexes.

With the emphasis on increasing opportunities for pedestrians in the corridor, buildings will increasingly be viewed from a pedestrian's point of view. This means that greater detail and greater articulation of building features are warranted, particularly in the TOD and CAC Districts. Articulated building footprints and facades reduce the perceived volume or massing of the building and promote architectural interest.

Flex space buildings are common in the Route 1 corridor and are a permitted use in the new CE District. Their one-story design and often long, undifferentiated building mass may not make an attractive or strong visual impact from the road. To add visual interest to this type of building, the building facades should be articulated with architectural design details.

### ***Recommendations:***

1. Design new multistory buildings with three distinct components: base, middle and top (Figure 5.5). Define each component by horizontal and/or vertical articulation. Facade articulation may consist of changes in the wall plane, use of openings and projections or protrusions from the wall plane, and material and color variations. Integrate parking structures with their associated buildings. If the parking structure is freestanding, apply the same design concepts listed here.
2. Introduce exterior articulation such as changes in color, material or plane for facades that are visible from public streets, if the building's function requires a basic, box-like building form. If the facade is more than 100 feet in length, use recesses and offsets, angular forms or other features to provide visual interest (Figure 5.6).
3. Articulate the corner on corner buildings. Treat corner entry architecture distinctively, to enliven intersections and facilitate pedestrian flow around the corner. Examples of treatments include angled or rounded corners, corner entries, towers,

arches, and other architectural elements (Figure 5.7 and 5.8).

4. Provide the greatest visual interest at the building base, especially in the pedestrian-oriented areas. Articulate the building materials and openings. Carefully design the rhythm, or repetition, of windows, columns, indentations or projections in the facade to create a balanced pattern.
5. Avoid large expanses of highly reflective surface and mirror glass exterior walls, to prevent heat and glare impacts on the adjacent public streets and properties.
6. Avoid bright colors, particularly primary colors; limit them to trim and accents. Avoid bright white or off-white for nonresidential buildings.

## Roof Design

### *Goals:*

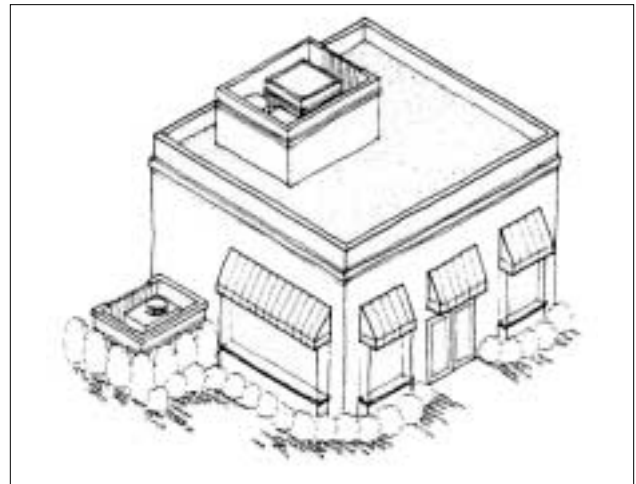
Building roofs should be an integral part of the building design. They are an important element contributing to the visual interest of a building. Roofs should also be designed to screen rooftop mechanical equipment.

### *Requirements:*

1. Integrate parapets and roof screens into the roof design of new buildings and new additions. The material and color of roof screens shall appear identical to those in the roof or building (Figure 5.9).
2. Screen on-roof mechanical equipment by solid building elements. Wherever possible, cluster roof equipment and include in one screen.

### *Recommendations:*

1. Break up long horizontal roof lines by providing articulations in the facades of new buildings. These articulations include change in the height or slopes of portions of roofs, or change in color, material, forms, etc. (Figure 5.10).
2. Encourage the use of equipment wells for sloped or pitched roofs.



*Figure 5.9. Parapets and roof screens help hide mechanical roof equipment.*



*Figure 5.10. Minor changes in roof heights, materials and details can break up long building facades.*

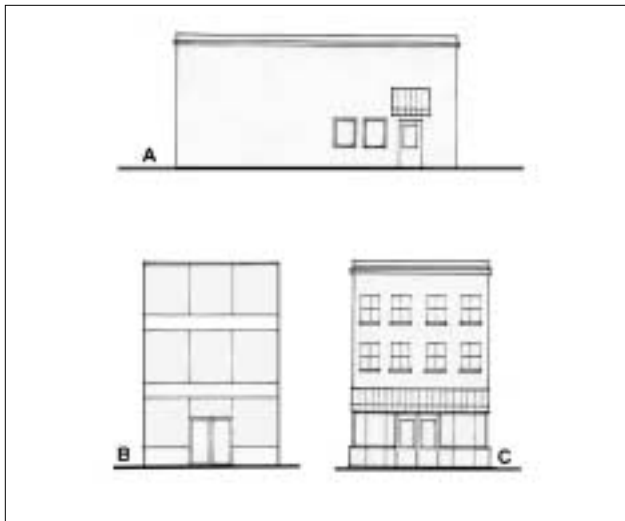


Figure 5.11. A. The doors and windows are unimportant features in this facade and are uninviting features to pedestrians. B. Although this facade offers visibility, the overall design is not animated or interesting. C. The repetition of windows and the balanced design of this facade is pleasing and attractive.



Figure 5.12. Ground floor windows with views of merchandise provide interest for pedestrians and attract customers.

3. Consider the use of decorative roof elements, such as projecting cornices, to enhance roof edges especially for mixed-use buildings and buildings that include retail uses.
4. For large or tall buildings, set roof-mounted mechanical equipment back from the edge of the roof so that it is not visible from the public right-of-way. Such setbacks may make roof screens unnecessary.

## Door and Window Openings

### Goals:

Windows and doors in the facade provide visual interest, encourage pedestrian use and improve safety. Especially in pedestrian-oriented areas, buildings should have doors and first floor windows facing the streets. People using the doors and looking at and through windows on the street sides of buildings will help to enliven the streetscape. Pedestrian activity can help make streets and the buildings that line them safer. Therefore, the placement of doors and windows is an important element for designing an attractive and well-functioning streetscape.

### Requirements:

1. In the TOD and CAC Districts, provide street-oriented primary entrances (Figure 5.11). Although an entrance on the public street side of the building is preferred, this requirement may also be satisfied by placing the major entrance on the side of the building, provided the entrance is visible from the street and connected to the street by a sidewalk. Dual access is also acceptable, with entries oriented to the street and to parking at the back or side of the building.
2. In the TOD and CAC Districts, maximize transparency and windows on the ground floor for pedestrian interest (Figure 5.12).

### Recommendations:

1. Avoid windowless walls along the public street.
2. Establish recesses for entries and for outdoor eating or display areas. Besides providing gathering areas, these recesses can create visual interest

along the street. Planters or landscaping may be incorporated into such recesses, but must not extend into the public sidewalk or right-of-way.

## Signs Attached to Buildings

### *Goals:*

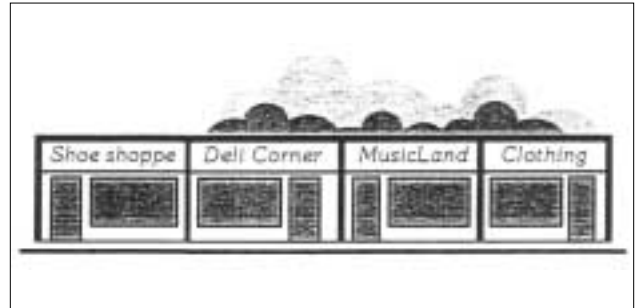
Wall and projecting signs should be integrated into the building design to minimize visual clutter and help achieve a more pleasing view from public streets (Figure 5.13).

### *Requirements:*

1. Comply with the requirements of the Howard County Sign Code (Subtitle 5).

### *Recommendations:*

1. Encourage the use of canopies to display the names of businesses.
2. Encourage the use of similar colors and styles in the signs and in the building facades.
3. Use durable materials; limit the use of bright colors and use professionally executed graphic design.
4. Limit signs in windows to no more than 25% of the total window glass area of each retail tenant on the first floor facing the street. Window signs are limited to retail uses only, not for office and industrial uses (Figure 5.14).



*Figure 5.13. Locating signs on canopies or on special wall-mounted sign panels provides a consistent and attractive appearance.*



*Figure 5.14. Signs on windows need to be limited so that they don't distract from the building itself and so that store interiors remain visible to pedestrians.*

## APPLICATION OF THESE STANDARDS

*Purpose: Because these requirements and recommendations will apply both to new developments and to existing developments as they are changed, it is important to clarify when they will apply completely and when they will apply only partially. Although it is not possible to cover all situations, this chapter provides guidance in making these distinctions.*

### Introduction

The requirements and recommendations of this Manual seek to augment and amplify the Zoning Regulations for the CE, TOD and CAC Districts. Compliance with this Manual's requirements and recommendations will, ideally, result in a more complete realization of the intent of the Zoning Regulations. However, if the Department finds extraordinary hardships or practical difficulties with strict compliance with the requirements of this Manual, or if the Department determines that the purposes of this Manual may be better served by an alternate proposal, then a modification to the requirements of this Manual may be granted.

Applicants seeking modifications to these requirements shall submit a written request to the Department of Planning and Zoning specifying the section of this Manual they propose to fulfill through alternative compliance. The request must demonstrate that:

1. Strict compliance with the requirements is not feasible or practical or
2. An attractive alternate means of compliance is beneficial or preferred.

### New Development

All new development in the three new districts must comply with the applicable requirements listed in Chapters 3, 4 and 5 of this Manual. New development in all districts on sites with Route 1 frontage must comply with the applicable requirements listed in Chapter 3. Compliance with applicable recommendations listed in this Manual is strongly encouraged.

### Exemptions

As mentioned above, many parcels in the corridor were developed before these districts were created. Development anticipated by these districts will be achieved over time by bringing existing sites into compliance with these requirements and recommendations as uses are altered, expanded or redeveloped. It is not the intent of these new regulations and this Manual to impose an undue hardship on existing structures and uses that propose minor improvements. Thus, the following minor alterations or enlargements are exempt from complying with the Manual:

1. Expansion of a building by less than 10% of the floor area of the building, as existing on the effective date of this legislation, up to a maximum of 5,000 square feet of floor area.
2. Building repairs, repaving or restriping of parking areas, and other maintenance or repair that does not enlarge a building or a use.
3. Removal of parking areas, driveways or other paved areas.
4. A change in the use of an existing building (to a use permitted in the district), if the Department of Planning and Zoning determines, in accordance with the Subdivision and Land Development Regulations, that no changes to site improvements are required.
5. Other minor alterations to a developed site that do not require a site development plan or a revision to an approved site development plan. This includes alterations approved through a waiver of the site development plan requirement or a red-line revision to an existing site development plan.

## Existing Improvements

Because much of the development in the corridor occurred prior to the establishment of the three new zoning districts, treatment of existing uses is important. Although the CE, TOD and CAC Districts are intended to encourage redevelopment, existing uses may continue and make some changes and enlargements without upgrading the entire site or building to meet the new standards.

Nonconforming uses and noncomplying structures cannot use the bulk regulations for the CE, TOD and CAC Districts unless the site is brought into compliance with the requirements of this Manual as determined by the Department of Planning and Zoning (DPZ). Those sites that DPZ determines cannot be brought into full compliance should use the bulk regulations stipulated in the Zoning Regulations for sites not complying with the regulations.

Existing warehouse and industrial buildings in the CE and CAC Districts that were developed for these uses prior to the adoption of the CE District may continue through the use of the Continuing Light Industrial (CLI) Overlay District. The existing warehouse or industrial buildings should be documented with an approved site development plan. If necessary, other materials such as aerial photography may be used to document the existing buildings. Improvements of up to 25% of the square foot area are allowed by right and must meet the bulk regulations and other requirements of the Route 1 Manual for the applicable CE or CAC District.

Alterations or enlargement of existing uses, except as described above, must comply with the Manual. The extent to which they must comply, however, will be in proportion to the extent of the expansion. If buildings or site improvements are expanded, the site shall be brought into compliance with the Route 1 Manual in equal proportion to the percentage of the site impacted by the expansion. The area impacted by the expansion includes the square foot area of building additions and additional parking, loading, driveways, infrastructure, and land cleared or graded.

Site improvements that do not change existing buildings only need to address the Site Renovation and Expansion priorities listed below. Building expansions that do not change site improvements only need to address Building Renovation and Expansion priorities.

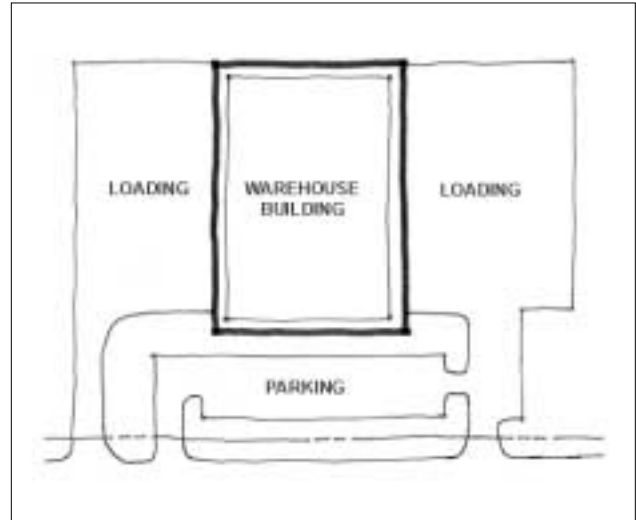


Figure 6.1. **Before.** Building and parking comply with current M-1 setbacks, parking is in front of the building and multiple driveways provide site access.

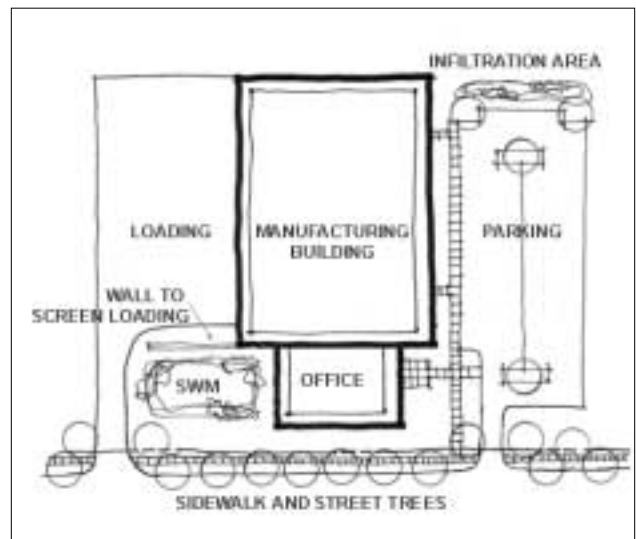


Figure 6.2. **After.** Building is converted to office and manufacturing uses, building and parking comply with CE setbacks, side entrance to building is oriented to roadway, number of driveways is reduced, some loading is converted to parking and loading is screened.

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When both site and building expansion occurs, both sets of priorities should be addressed (Figures 6.1 and 6.2).

### ***Site Renovation and Expansion***

When partial improvements are made to a site, priority improvements, generally in the order given, are as follows:

1. Add a public sidewalk and construct an on-site sidewalk that links the public sidewalk to the building entrance.
2. Channelize or reduce the number of access points and provide planting, signage and other features to accent the site entrance.
3. Provide street trees.
4. Eliminate outdoor storage areas or screen the view of loading and outdoor storage areas from the Route 1 roadway and other major roads, and from adjacent residential properties.
5. Use planting or low walls to buffer parking areas from the street.
6. Add islands and interior planting to existing parking lots.
7. Remove pole mounted signs and replace with low profile, monument type signs.

### ***Building Renovation and Expansion***

Relocating or reconstructing existing buildings is not required, but expansions of buildings should attempt to locate and design the addition to be consistent with the requirements and recommendations in this Man-

ual. For building expansions, priority building improvements, generally in the order given, are as follows:

1. Expand the building so that the addition brings the building closer to the Route 1 roadway or to the setback from the design right-of-way established in the new district.
2. Expand the building by adding floors to the existing building or by constructing a multistory addition.
3. Provide a main entrance to the building that faces the Route 1 roadway or provide a side entrance that is visible from the Route 1 roadway.
4. Ensure that there are windows along the street facade.
5. Install new or improve existing wall mounted signage, bringing it into compliance with the sign code or making it more compatible with building materials and design.
6. Screen roof or ground mounted mechanical equipment or utility boxes.

## **Interpretation**

Interpretation of this Manual is the responsibility of the Department of Planning and Zoning. Relief from any of the requirements of this Manual can be requested through an administrative process via a letter of justification for an alternative means of compliance. A zoning variance is not required. Relief from any of the subdivision regulations can be requested through an administrative process via a waiver petition. Refer to Section 16.104 of the Howard County Subdivision and Land Development Regulations.





# APPENDIX A

*This appendix presents an overview of the key requirements and recommendations for properties in the CE, TOD and CAC Districts and/or that are adjacent to the Route 1 right-of-way. Please note that not all requirements and recommendations are listed in the appendix. The appendix should be used for general information only. For specific requirements and recommendations, please refer to the appropriate chapter in this Manual.*

Summary Matrix				
Key Requirements and Recommendations	CE	TOD	CAC	OTHER*
<b>STREETSCAPE DESIGN</b>				
With roadway improvements, provide on-street parking		○	○	
Construct interconnecting streets; avoid cul-de-sacs	○	○	○	○
Along Route 1 and pedestrian access roads to MARC stations, install 15' wide sidewalks and pedestrian street lights		●	●	
Install 5' wide sidewalks and, where adequate r-o-w exists, provide 6' wide green strip next to sidewalk (for streets where 15' wide sidewalks are not required)	●	●	●	●
Provide crosswalk pavement treatment	●	●	●	●
Where sidewalk must be next to curb, place 2' wide band of concrete pavers next to curb	○	○	○	○
Along Route 1, provide trash receptacles, benches and bollards and provide space for bus shelters if requested	○	○	○	○
Create plazas, courtyards and gardens	○	○	○	
Locate utility cabinets and mechanical equipment away from streets and residential areas; otherwise, screen them	●	●	●	●
Place utility lines underground or provide underground conduits for future utility location	○	○	○	○
Plant medium/large shade street trees 40' apart	●	●	●	
Establish a 100' r-o-w for Route 1, except where it must be greater, as determined by SHA & DPZ	●	●	●	
<b>SITE DESIGN</b>				
Establish zero foot build-to line where applicable		●	●	
Occupy 75% of applicable frontage with buildings		●	●	
Occupy at least 30% of Route 1 frontage with buildings	●			
Minimize curb cuts	●	●	●	○

**LEGEND:** ● Required ○ Recommended

\* **OTHER:** Properties in all districts that abut the Route 1 right-of-way.

Key Requirements and Recommendations	CE	TOD	CAC	OTHER*
<b>SITE DESIGN, Continued</b>				
Share entrances	○	○	○	○
Locate parking at the side or rear of buildings	●	●	●	○
Link adjacent commercial properties by common drive aisle	○	○	○	
Provide walkways from parking lots to building entrances	●	●	●	
Create an outdoor amenity area with at least 5% of the net site area in CE and 10% in TOD and CAC	●	●	●	
Add low walls at main entrance to parking lot	○	○	○	
Distribute landscaping throughout parking lot; landscape islands and medians	●	●	●	
Screen all outside storage areas	●	●	●	○
Provide pedestrian amenities such as benches, planters, etc.	○	●	●	
Prohibit loading at front of building	●	●	●	○
<b>BUILDING DESIGN</b>				
Build to a minimum height of 25'			●	
Articulate building facades to provide visual interest	○	○	○	
Integrate mechanical equipment screens into roof design	●	●	●	
Provide street-oriented primary entrances	○	●	●	
Provide windows on the ground floor	○	●	●	

**LEGEND:**      ● Required    ○ Recommended

**\* OTHER:**      Properties in all districts that abut the Route 1 right-of-way.

*Credits:*

The following publications and consultants were sources for images and text used in this document:

American Planning Association, PAS Report 468, Creating Transit-Supportive Land-Use Regulations

City of San Carlos, Downtown Urban Design Guidelines

College Park US 1 Corridor Sector Plan, Prince George's County, M-NCPPC

Design Collective, Inc. / Maple Lawn, Maryland, developed by Greenebaum and Rose, Associates

Greenman-Pedersen, Inc.

Internet Web Sites

URS Corporation

Visions for a New American Dream, A.C. Nelessen, APA Planners Press



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